

DESIGN REVIEW PERMIT ANALYSIS

As noted above in the Coastal Development Permit (CDP) analysis, the proposed project consists of construction and operation of a 16,157-square-foot, one-story, retail store with a 55-space parking lot and associated improvements, landscaping, and infrastructure. The proposed visual conditions are shown in the visual analysis prepared for the Environmental Impact Report. The project is subject to Design Review per Section 17.71.050 of the CLUDC and must conform with the Citywide Design Guidelines.

The following analysis considers if the proposed project conforms with design review criteria and the Citywide Design Guidelines as well as the findings required to approve the Design Review Permit.

A grocery outlet franchise typically uses the following standard design for their storefronts.

Figure 1: Typical Grocery Outlet Design



However, this standard design does not comply with the Citywide Design Guidelines. Therefore the applicant was asked to develop a design that complies with the Citywide Design Guidelines. The submitted design is illustrated in the photos on the following page and in Attachment 3: Grocery Outlet Floor Plan Elevations.

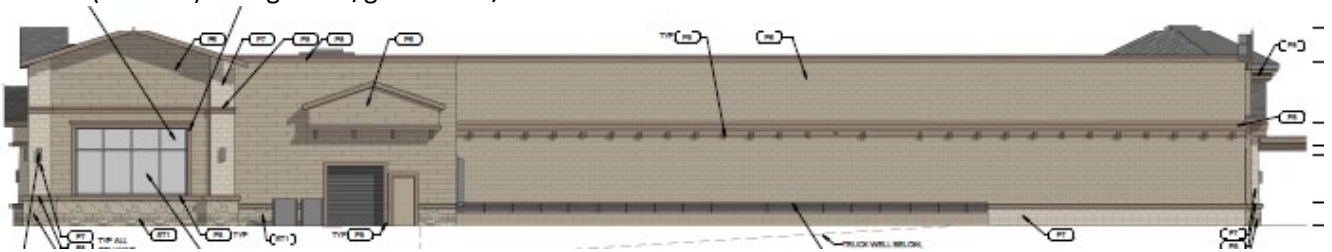
S. Franklin St. Elevation:



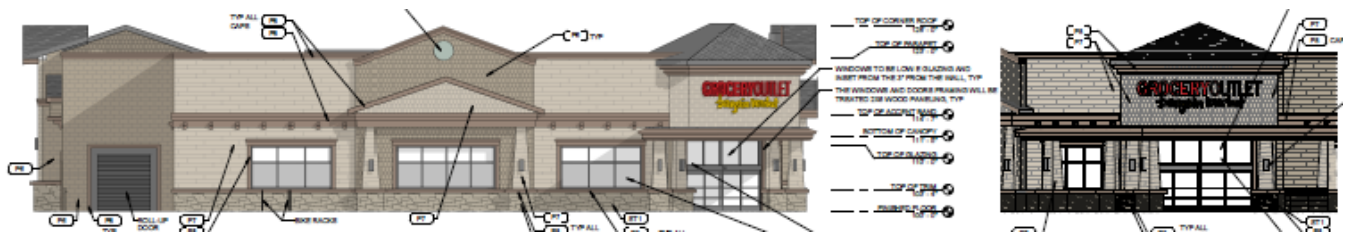
South St. Elevation:



Backside (Internal) facing fence/gas station/Taco Bell Elevation:



N. Harbor Drive Elevation:



Additionally, the visual simulation (Attachment 8 and below) illustrates how the building would appear onsite.

View 1: From the corner of South Franklin and N. Harbor



View 2: From intersection at South St. and S. Franklin St.



View 3: From South St.



Design Review Findings. The Planning Commission must evaluate the application to ensure that the project complies with the following findings in order to approve a Design Review permit.

1. Complies with the purpose and requirements of this Section (Design Review in the CLUDC).
2. Provides architectural design, building massing, and scale appropriate to and compatible with the site surroundings and the community.
3. Provides attractive and desirable site layout and design, including building arrangement, exterior appearance and setbacks, drainage, fences and walls, grading, landscaping, lighting, signs, etc.
4. Provides efficient and safe public access, circulation, and parking.
5. Provides appropriate open space and landscaping, including the use of water efficient landscaping.
6. Is consistent with the General Plan, and applicable specific plan, and the certified Local Coastal Program.
7. Complies and is consistent with the City's Design Guidelines.

As mentioned above the Design Review process requires substantial compliance with the Citywide Design Guidelines. This includes the four guiding principles of the Citywide Design Guidelines (analyzed below) and the mandatory and preferable Design Guidelines (analyzed later by component).

*Guiding Principle 1: **Community Character***

Project design should reflect and strengthen the distinct identity of Fort Bragg – a rural, historic small town on the Mendocino coast.

The proposed project design has features that are compatible with, without trying to mimic historic design, including parapets and building articulation which break up the building's massing. It is similar in design quality to other recently constructed large format and franchise stores such as CVS, McDonalds and Taco Bell. It has better design character than some larger franchise stores which were

constructed prior to the adoption of the Citywide Design Guidelines, such as Safeway, Pizza Hut and RiteAid.

*Guiding Principle 2: **Support Connectivity***

Project design should incorporate safe, functional and multimodal connections that are easy to navigate by walking, bicycling and public transit. When feasible, new streets should follow existing development pattern.

The proposed project would result in the construction of new sidewalks on a parcel which currently lacks sidewalks. A bus stop is located across the street from the project. The project includes bicycle racks and easy access to the Class II bicycle lane on Franklin Street.

*Guiding Principle 3: **Public Enhancements***

Project proposals should positively enhance the adjacent public realm by contributing to the collective good of community. This means building places, and not individual sites; making design consideration in the context of streets, sidewalks, public spaces, parks, and trails and looking at how the community interacts with these public spaces.

The project includes significant landscaping which would screen the parking lot from public view, while providing comfortable spaces to walk on new sidewalks. The project applicant made a design decision to build the proposed structure on the footprint of the existing structure, which means that the urban form will not change significantly on this block.

*Guiding Principle 4: **Water & Power Sustainability***

Do more with less. Development should incorporate water and power efficient design strategies.

As conditioned, the project incorporates permeable paving and bioswales to reduce stormwater flows and native plantings which require less watering. The project will achieve Title 24 energy efficiency in compliance with the State Building Code. The Planning Commission could recommend that the project incorporate solar as part of the Building Permit process. The proposed roof plan does not currently include solar panels although a location is reserved for them on the plans. Special Condition 20 can be recommended by the Planning Commission.

Special Condition 20: The building permit application plans shall include solar panels on the roof.

The Citywide Design Guidelines also include a specific design guideline for South Franklin Street as follows:

Franklin Street South

From the intersection of Oak and Franklin Street to North Harbor Drive lies the Franklin South Corridor. This corridor on the eastern side of the street is mainly an eclectic mix of single-family residences in a variety of building forms, setbacks, and landscape character. While the western portion is mainly made up of hotels and commercial development. Due to this mix of development, there is no significant architectural style and detail present throughout the corridor. Sidewalks and class II bikeways are present on both sides and speed limits are a maximum of 30MPH making it one of the more pedestrian friendly streets in town.

With some relatively large opportunity sites in this area, new development is likely to have a transformative impact. As new development occurs, new sites and buildings should be designed with the objectives listed below in mind.

- *Ensure a comfortable pedestrian environment through design approaches for a front setback area.*
- *Limit parking to the rear or alley of primary structures.*
- *Create a visual and physical connection between a building's entry and the public realm.*
- *Emphasis on front yard trees and landscaping.*
- *Mixed-use development is heavily encouraged.*

Project compliance with each of the above requirements is analyzed below:

- **Ensure a comfortable pedestrian environment through design approaches for a front setback area.**

The proposed project provides a 12' 9" setback along Franklin Street which is landscaped with a variety of shrubs and trees. The proposed sidewalk is 5 feet wide. In order to improve the sense of the public realm and the setback area, the Planning Commission can recommend special condition 21.

Special Condition 21: The sidewalk on South Franklin Street shall be 6 feet wide, and two benches shall be installed in the landscaped area parallel to and adjacent to the sidewalk.

- **Limit parking to the rear or alley of primary structures.**

The proposed project includes parking to the south of the structure that faces the building entrance. This is very common for grocery stores and other large format retailers, and indeed all the City's grocery stores front their parking lots. This is necessary to easily bring groceries from the store via cart to one's car. Due to parcel configuration (long and thin) the project site would not support parking at the rear of the parcel for any building equivalent to the existing structure in size. This is especially true for a grocery store as any grocery store would have to be too long and thin to work effectively as a grocery in order to accommodate all parking behind the building. Compliance with this design guideline is not feasible given the parcel configuration and the need for grocery cart accessibility.

- **Create a visual and physical connection between a building's entry and the public realm.**

The proposed project has a 12-foot-wide concrete plaza and entrance that connects the project to the Franklin Street sidewalk. This is a good physical connection. The building has many windows that face Franklin Street and S Harbor Drive which create good visual connection to the street.

- **Emphasis on front yard trees and landscaping.**

The project includes a large number of street trees on all site edges within the public realm. The "front yard of the project" along Franklin Street has 14 trees, while the front yard fronting N Harbor drive has 5 trees. The project has incorporated extensive front yard landscaping.

- **Mixed-use development is heavily encouraged.**

The proposed project is a single use (retail) development but it is part of a very mixed neighborhood which includes hotels, gas stations, restaurants and residential uses all located within the immediately surrounding blocks.

Chapter 2 Design Review Requirements

Additionally, the project must be reviewed for compliance with the requirements of Chapter 2 of the Citywide Design Guidelines. As conditioned, the project is in substantial conformance with these guidelines as follows:

Massing Elevations and Articulation – Mandatory Standards

The project addresses all mandatory standards as follows:

1. It is well articulated on the three sides that face the public right of way.
2. The scale of the building relates to the two-story development pattern of the hotel on the adjacent parcel. The building is essentially two stories in height and as a grocery store additional step backs are not feasible beyond the small amount that is achieved with the building footprint and massing.
3. Includes architectural detailing at the pedestrian level such as windows, building base materials change, awnings, trellises, and window murals.
4. The project does not include franchise architecture (Architectural Form & Detail #1)
5. The project incorporates some features from the historic downtown, namely windows and awnings (Architectural Form & Detail #2).

Roof forms – Mandatory Standards

The project complies with the mandatory standards for roof form with the exception of the items listed below.

1. The roof Parapet does not “include detailing typical of Fort Bragg’s character and design.” The proposed project does not use much architectural detailing on the parapet. The Planning Commission can recommend Special Condition 22.

Special Condition 22: The applicant shall submit a revised design that includes additional detailing in the parapets for consideration and approval by the Community Development Director.

Windows, Doors & Entries – Mandatory Standards.

The project complies with all mandatory standards for windows and doors except for storefront window requirements.

- Windows are incorporated at the storefront location and includes use of clear glass (at least 80% light transmission). However as proposed these windows would be painted with murals which would reduce light transmission significantly. Special condition 23, above, will address this issue.
- The size and location of doors and windows relate to the scale and proportions of the overall structure.

- The main building entrance is distinguished from the rest of the building and easily recognizable and oriented toward the internal walkway, street and parking lot.

Materials- Mandatory Standards

The proposed project complies with the mandatory materials list with one exception.

- The front façade includes the following materials for the exterior elevation from the Encouraged List: Hardi Board Composite, Wood Paneling, Hardi Board Composite Half, Round "Fish Scale" Paneling, Wood Roof Shingles.
- It also includes the following materials from the Acceptable List: Cultured Stone with an authentic appearance, and Country Ledgestone.
- However, the project includes Smooth Face CMU, which is considered a “discouraged” building material. The CMU is proposed for portions of the building fronting Franklin Street and South Street and the west face of the building which fronts the property line with the gas station.

Colors.

There are no mandatory standards for color. The proposed project would be painted with three different earth tones namely: Driftwood, Indian River and Smokey Taupe. The project complies with the following preferred standards for color:

- Colors enhance different parts of a building’s façade and are consistent with the architectural style.
- Colors visually relate building elements (trim, roof, pedestrian level wall) to each other. The colors complement neighboring facades.
- The building colors reflect the basic colors of the architectural style or period of the building. they are earth tone colors as required for the Coastal Zone.
- Two colors are included on every façade.



ELEVATION NOTES

1. ALL BUILDING HEIGHTS ARE ABOVE INTERIOR FINISH FLOOR NOT ADJACENT GRADES.

LEGEND

EXAMPLE IMAGE	GRAPHIC	DISRIPTION
		SMOOTH FACE CMU
		HARDI BOARD COMPOSITE WOOD PANELING
		HARDI BOARD COMPOSITE HALF ROUND "FISH SCALE" PANELING
		WOOD ROOF SHINGLES
		CULTURED STONE - COUNTRY LEDGESTONE
		P8 - DRIFTWOOD
		P6 - INDIAN RIVER
		P7 - SMOKEY TAUPE

Lighting - Mandatory Standards

Standard	Compliance
1) Exterior lighting shall be designed as part of the overall architectural style of the building and should illuminate entries, driveways, walkways, and activity areas.	Exterior lights is proposed as simple lighting boxes with downlighting. The lighting boxes are attached the side of the buildings. The lighting plan shows that the driveways, walkways and entry ways would be effectively illuminated.
2) Entrances shall be well illuminated for safety and identification purposes.	Please see Attachment 9 – Lighting Plan. The entrance will be well illuminated.
3) Lighting sources shall be hidden unless the sources are an integral part of the design. Lighting fixtures should not project above the fascia or roofline of the building.	Please see Attachment X – Lighting Plan. The lighting sources are integral to the design, all lighting fixtures are located well below the Fascia.
4) Partial or full cutoff lighting is required. Exterior lighting shall be located and designed to avoid shining directly onto nearby residential properties, and shall minimize off-site glare. The latest technical and operational energy conservation concepts should be considered in lighting designs.	Please see Attachment 9 – Lighting Plan. The project, as designed, would avoid shining light directly onto nearby residential properties.
5) Parking lot lighting fixtures shall be no taller than 16 feet in height and shall cast light downward without allowing glare or light to encroach upon neighboring properties	The Lighting plan illustrates parking lot lighting fixtures in excess of 16 feet in height. Special Condition 26 is included to address this. All fixtures are downward and do not allow glare to encroach upon neighboring properties.

Special Condition 26: The Building Permit plans shall illustrate parking lot lighting standards that are not taller than 16 feet in height.

Site Planning - Mandatory Standards

The proposed project complies with the mandatory site planning standards.

1. The proposed project has been sited to minimize impacts to surrounding development. The proposed use will be considerably more intensive than the existing use both in terms of operating hours and the number of vehicles and people coming to the site. However, by occupying the same footprint as the current building the proposed project would minimize new impacts to surrounding development. The project is not adjacent to open space and so will not have an impact on open space. The proposed project is on a flat lot without natural areas and so the mandatory requirement “to place structures well to minimize impacts to natural areas and natural contours” does not apply.
2. The proposed project complies generally with the second mandatory standard: “Buildings should generally be oriented toward the street. Buildings on corner parcels should establish a strong tie to both streets.” The front of this building is oriented toward Noyo Harbor Drive with a strong secondary orientation to Franklin Street via the plaza and architectural features.

The project generally complies, as conditions with the preferred Site Planning standards, as the building is oriented to the South to take advantage of solar access for passive and active energy needs and to moderate the impact of prevailing winds which come from the North.

Landscape - Mandatory Standards

The project complies with the mandatory landscaping standards.

1. As conditioned the project does not include plants and trees with root systems that could uplift hardscape materials. Specifically Special Condition 1 requires the applicant to select an alternative tree type.
2. As conditioned the landscaping plan will use trees and plants native to the Northern California coast.

Fencing and Screening - Mandatory Standards

The proposed project plans does not include sufficient detailed information to determine if the design complies with the following mandatory requirements for fences:

1. “Fences or walls of more than 100 ft should provide variation in the design – via changes in height, materials, embellishments, step backs, gates, etc. - to break up the length and provide visual interest.”

Therefore, the Planning Commission may recommend Special Condition 27.

Special Condition 27: Prior to approval of the Building Permit application, the applicant shall provide an elevation of the new fencing/sound wall from both the east and west perspective. Further the community Development Director shall ensure conformance with the design guidelines related to fencing.

The proposed project does not comply with the second Mandatory requirement as the project fence/sound wall would result in hiding places or entrapment areas by the loading dock. The public interest in health and safety may be better served by keeping people out of the loading dock area than

providing a gate to the adjacent property at this location. The Planning Commission should request Special Condition 28, if egress is more important.

Special Condition 28: The Building Permit application shall include an exit gate by the loading dock to facilitate emergency egress out of the loading area.

Site Amenities - Mandatory Standards

The proposed project does not include more than one unit (retail store) so the mandatory unit numbering, guest parking, and other requirements of this guideline do not apply to the proposed project.

Pedestrian Circulation - Mandatory Standards

Pedestrian access connects buildings to their surroundings and encourages street activity. This project must add a “drop off only” signage and white marking space along the Franklin Street Frontage parallel to the Building entry to comply with the only mandatory guideline in this section. Special Condition 29 is included to achieve *this objective*.

Special Condition 29: *The applicant shall install a Pick-up/ Drop-off Sign on Franklin Street adjacent to the Entryway. This area will include at least two spaces that are painted for 10-minute pick up and drop off.*

The project does not comply with the preferred standard to have “continuous, clearly marked pathways from the parking areas to main entrances of buildings” nor has the sidewalk been designed to “minimize pedestrians crossing parking stalls and landscape islands to reach building entries.” However, given the parcel geometry and the minimum 8’ width of landscaping required between the sidewalk and the parking lot it is not feasible to add pedestrian only paths of travel to the interior of the parking lot. This level of pedestrian access is not provided in any of the other large format stores in Fort Bragg.

Circulation and Parking - Mandatory Standards

The proposed project complies with the Mandatory circulation and parking standards as the lot is “well designed, with consideration given to landscaping, lighting, building massing, and pedestrian/vehicular circulation” and is “designed for safe ingress and egress”.

Loading and Delivery - Mandatory Standards

The loading and delivery service area complies with the mandatory standards as the loading area is located at the rear of the building to minimize its “visibility, circulation conflicts, and adverse noise impacts.” Additionally, the proposed loading and delivery areas is “screened with portions of the building, freestanding walls and landscaping planting.”

Design Review Findings

As previously mentioned, the Planning Commission must evaluate the application to ensure that the project complies with the Design Review Finding as analyzed above and below.

1. Complies with the purpose and requirements of this Section.

This finding can be made, because as conditioned (discussed in detail above), the project complies with the purpose and mandatory requirements of the Citywide Design Guidelines.

2. Provides architectural design, building massing, and scale appropriate to and compatible with the site surroundings and the community.

This finding can be made, because as conditioned (discussed in detail above), the project provides architectural design, building massing and scale that is compatible with the site surroundings and community. Specifically, the building size and massing are permissible with the site zoning and similar to that of other hotels and large format grocery stores in the neighborhood. The level of architectural design is significantly better than many of the other structures in the neighborhood

3. Provides attractive and desirable site layout and design, including building arrangement, exterior appearance and setbacks, drainage, fences and walls, grading, landscaping, lighting, signs, etc.

Compliance with the adoptions of the listed special conditions and the Cityside Design Guidelines and the CLUDC as detailed above ensure that this finding can be made.

4. Provides efficient and safe public access, circulation, and parking.

As previously discussed in this report, the project has been designed and conditioned to provide efficient and easy pedestrian and vehicular circulation and parking.

5. Provides appropriate open space and landscaping, including the use of water efficient landscaping.

As conditioned the project provides sufficient landscaping to comply with the CLUDC and the Cityside Design Guidelines.

6. Is consistent with the General Plan, and applicable specific plan, and the certified Local Coastal Program.

As analyzed and conditioned in this report and as mitigated in the EIR this project is consistent with the Coastal General Plan and the CLUDC which together make up the Local Coastal Plan.

7. Complies and is consistent with the City's Design Guidelines.

As conditioned above, the project is consistent with the mandatory requirements of the City's Design Guidelines.

SIGN REVIEW PERMIT ANALYSIS

The sign plans are in Attachment 10. Pursuant to Section 17.38.040 of the Coastal Land Use and Development Code, the review authority must make all of the following findings.

- 1. The proposed signs do not exceed the standards of Sections 17.38.070 (Zoning District Sign Standards) and 17.38.080 (Standards for Specific Sign Types), and are of the minimum size and height necessary to enable pedestrians and motorists to readily identify the facility or site from a sufficient distance to safely and conveniently access the facility or site;**

The proposed channel sign on the building and the monument sign proposed for the southeast corner of the lot comply with the standards in 17.38. Both signs comply with height limits. The wall sign is 83.3 square feet and the proposed entry sign is 30 SF on each side for a total of 60 SF. Only one side of the free standing sign is used in the total signage calculation. The total signage for the site is therefore 83.3 SF + 26 SF = 109.3 SF. This is 9.3 SF more than the allowed maximum of 100 SF. The proposed sign does not include the site address number as required by the CLUDC. Planning Commission may recommend Special Condition 30 below:

Special Condition 30. Prior to approval of the Building Permit the applicant shall submit a revised sign plan that includes no more than 100 SF of signage, and the monument sign shall include the required site address, and substantially replicate the proposed sign design and locations, for approval by the Community Development Director.

- 2. That the placement of the sign on the site is appropriate for the height and area of a freestanding or projecting sign;**

The placement of the sign on the building facade is appropriate for the height of the building. The placement of the 6-foot-tall monument standing sign as proposed is not appropriate because the monument sign is located in the traffic safety visibility area which is measures 20 feet in each direction from the corner of the lot (not from the corner of the stop bar as noted on the plan set). **Special Condition 31** maybe recommended to address this issue.

Special Condition 31: Prior to issuance of the Building Permit, the applicant shall submit a revised sign site plan, to be approved by the Community Development Director. The revised sign plan must illustrate that the monument sign is 20 feet back from the edge of the sidewalk in every direction (due to curved sidewalk situation) and is perpendicular to the street at its placement.

- 3. That a flush or projecting sign relates to the architectural design of the structure. Signs that cover windows, or that spill over natural boundaries, and/or cover architectural features shall be discouraged;**

The proposed flush building sign is a key component of the architectural design and related well to the design and the building entry.

4. **The proposed signs do not unreasonably block the sight lines of existing signs on adjacent properties;**

Proposed signs would not block the sight lines of any existing signs on adjacent properties.

5. **The placement and size of the sign will not impair pedestrian or vehicular safety;**

As noted previously noted the freestanding sign is proposed to be located within the traffic safety visibility area, which would be addressed by Special Condition 32.

6. **The design, height, location, and size of the signs are visually complementary and compatible with the scale, and architectural style of the primary structures on the site, any prominent natural features on the site, and structures and prominent natural features on adjacent properties on the same street.**

The heights, locations and sizes of the proposed signs, as conditioned, are adequately compatible with the scale and architectural style of the building.

7. **The proposed signs are in substantial conformance with the design criteria in Subsection 17.38.060.F (Design criteria for signs).**

The proposed signage complies with the mandatory standards for signs of Chapter 5 of the Citywide Design Guidelines. Specifically the proposed sign “relates to the architectural features of the building” and “coordinates with the building design, materials, color, size, and placement.” Additionally, as the proposed sign is the logo and trademark of Grocery Outlet, the City is limited in its ability to modify type face, lettering, spacing or similar sign characters.

The proposed sign also complies with the City’s mandatory standards in the Design Guidelines with regard to sign placement, color, materials, wall signs, illumination, and monument signs.