



# Pavement Management Update 2021 Final Report

NCE Project No. 270.09.55  
January 2022



**Richmond, CA**  
501 Canal Blvd., Suite I  
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## City of Fort Bragg

416 North Franklin Street  
Fort Bragg, CA 95437

**Final Report**  
**Pavement Management Program Update 2021**  
**City of Fort Bragg**

January 2022

**Prepared for:**

**City of Fort Bragg**

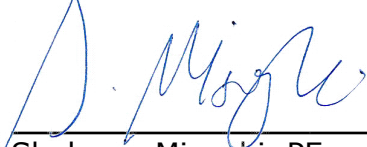
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## Executive Summary

In 2021, Nichols Consulting Engineers, Chtd. (NCE) was selected by the Mendocino Council of Governments (MCOG) to update the City of Fort Bragg's (City) pavement management program (PMP) for the entire network. This report summarizes the results of the 2021 update for the City and its purpose is to help educate policy makers about the current condition of the pavement network and the impact of various funding scenarios on future network conditions.

The City's pavement network consists of 30.6 centerline miles of paved streets and alleyways, which represents a substantial investment of approximately \$85.4 million. In 2021 NCE collected pavement condition data throughout the network using the Metropolitan Transportation Commission (MTC) survey protocols. The survey data were entered into the StreetSaver® database, which the City uses as a PMP decision-support tool.

Overall, the City's entire pavement network is currently in "Fair" condition with an average pavement condition index (PCI) of 65. Entire pavement network includes alleyways. The alleyways were added to the database and analysis this year for the first time. The PCI of the alleyways and streets (without alleyways) are 25 and 68, respectively. Overall, approximately 55.7 percent of the network is in "Good" condition and 25.2 percent is in "Poor" or "Failed" condition.

The budget needs analysis indicated that the City needs to spend approximately \$25.3 million over the next ten years to bring the street network to a condition that can be maintained with on-going preventive maintenance in the most cost-effective way. Out of \$25.3 million, the alleyways would need approximately \$5.3 million. Two alternative budget scenarios were performed to illustrate the impacts of different funding levels. The following table lists each scenario with its corresponding ten-year budget, PCI, and deferred maintenance at the end of the analysis period.

Scenario	Description	10-Year Budget (\$M)	2031 PCI	2031 Deferred Maintenance (\$M)
1	Existing Funding	\$10	58	30
2	Improve PCI to 75	\$26.7	75	11

NCE recommends that the City pursue Scenario 2, which will improve the existing overall network PCI to 75 by 2031. Additionally, under this scenario, the alleyways and the streets both will be in "Good" condition with PCI of 86 and 74, respectively. This scenario will increase the portion of the network in "Good" condition and limit the deferred maintenance. This will require \$26.7 million over the next ten years.

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## 1 Introduction and Background

In 2017, NCE assisted City of Fort Bragg in updating the StreetSaver<sup>®</sup> pavement management program (PMP) with pavement condition surveys, historical maintenance and rehabilitation (M&R) records, pavement strategies, and budgetary analysis. No update has been conducted on pavement condition surveys or maintenance and rehabilitation strategies since 2017. In 2021, Nichols Consulting Engineers, Chtd. (NCE) was selected by the Mendocino Council of Governments (MCOG) to update the City of Fort Bragg's (City) pavement management program (PMP) for the entire network. The alleyways were also added to the database during this survey.

In general, PMPs are "designed to provide objective information and useful data for analysis so that... managers can make more consistent, cost effective, and defensible decisions related to the preservation of a pavement network."<sup>1</sup>

This report answers the following questions for the City of Fort Bragg:

- What does the City's pavement network include?
- What is the current condition of the pavement network?
- What are the City's current M&R strategies?
- How much funding is required to perform all needed M&R treatments over the next ten years?
- What effect will the City's existing funding have on the network condition and deferred maintenance?
- What effect will other funding levels have on the network condition and deferred maintenance?

To update the City's PMP, NCE performed walking surveys using the Metropolitan Transportation Commission's (MTC) survey procedures<sup>2</sup>. Surveys did not include non-pavement issues such as traffic, safety and road hazards, geometric issues, shoulders, sidewalks, curb and gutters, drainage issues, or immediate maintenance needs.

All survey data were then entered into the City's StreetSaver<sup>®</sup> database and pavement condition index (PCI) calculations were performed. NCE then met with the City staff and reviewed and updated the maintenance and rehabilitation (M&R) strategies and treatments. NCE also updated the treatment unit costs based on recent bid tabs from the City.

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<sup>1</sup> AASHTO "Guidelines for Pavement Management Systems". American Association of State Highway and Transportation Officials, Washington, DC, July 1990.

<sup>2</sup> PCI Distress Identification Manuals (AC 4th Edition, PCC 3rd Edition), Metropolitan Transportation Commission, San Francisco, CA March 2016.

A budget needs analysis was performed for a ten-year analysis period with an annual inflation rate of 3 percent. This analysis identified M&R recommendations for each pavement section and determined the total M&R budget needs for the analysis period. Finally, two budget scenarios were analyzed for the street network.



## 2 Network Summary

The City is responsible for maintaining approximately 30.6 centerline miles of paved streets (or 239 pavement sections). The entire network is mostly composed of asphalt concrete (AC) pavement. Table 1 summarizes the street network by functional classification. Thirty-two (32) gravel streets, out of which thirty are alleyways, are also listed in the database but are not included in the budgetary analysis.

*Table 1. Network Summary Statistics*

Functional Class	Number of Sections	Centerline Miles	Lane Miles	Network Area (%)
Arterial	14	3.10	6.30	12.6%
Collector	40	7.80	15.20	32.0%
Residential	121	14.50	28.60	47.4%
Other (Alleyways)	64	5.2	10.3	8.0%
<b>Total</b>	<b>239</b>	<b>30.6</b>	<b>60.4</b>	<b>100%</b>
Gravel	32	2.23	4.46	-

The street network replacement cost is estimated to be approximately \$85.4 million. This can be viewed as the value of the pavement network and is the amount needed to fund reconstruction of the entire paved network. It does not include related infrastructure assets such as sidewalks, signals, markings, signs, or storm drains.

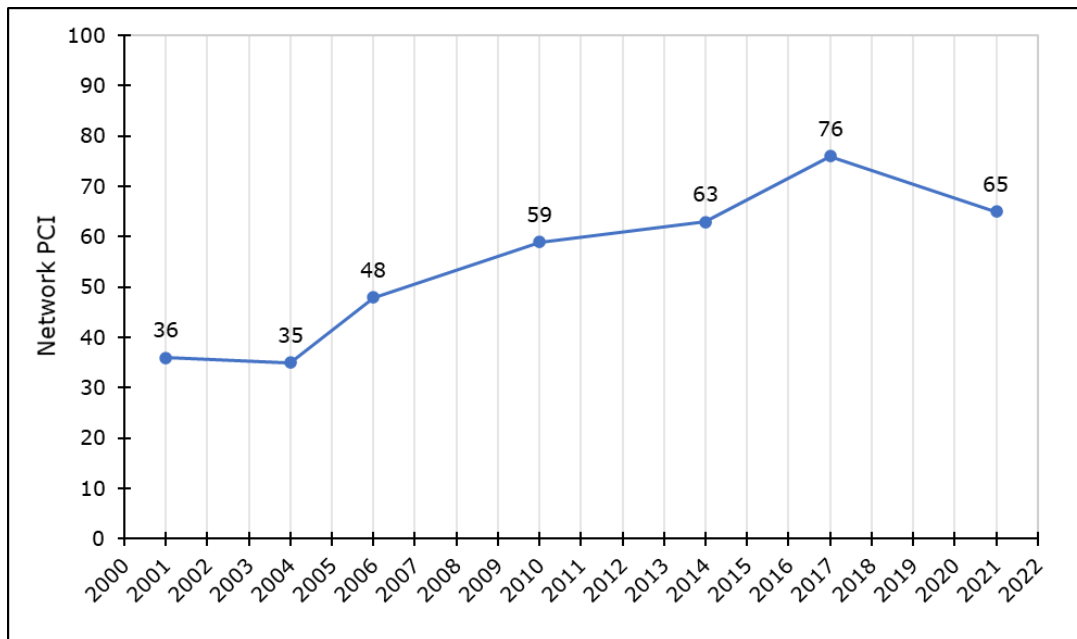
### 3 Pavement Condition

Pavement condition is typically quantified using the pavement condition index (PCI), which ranges from 100 (best) to 0 (worst). A newly constructed street has a PCI of 100, while a failed street has a PCI of 25 or less. Pavement condition is affected by the environment, traffic loads and volumes, construction materials, and age.

The PCI scale is divided into four general condition categories. Pavements in “Good” condition have a PCI above 70, pavements in “Fair” condition have a PCI between 50 and 69, pavements in “Poor” condition have a PCI between 25 and 49, and finally pavements in “Failed” condition have a PCI below 25.

#### 3.1 CITY’S PAVEMENT CONDITION INDEX

**The current average PCI for the City’s entire pavement network is 65 (this includes alleyways).** The current average PCI for streets and alleyways is 68 and 25, respectively. This value is an area-weighted calculation performed in StreetSaver® and is based on the condition survey performed in 2021. Figure 1 illustrates the City’s historical network PCI since 2001 which shows that the average network PCI for the City has increased by 29 points since 2001, which indicates treatment commitment made to repair and maintain the street network. Note that, the alleyways were inspected for the first time in 2021 and the current PCI reported in Figure 1 is for the pavement network including the alleyways.



**Figure 1. Historical Network PCI since 2001**

Figure 2 shows photos of City streets with varying PCIs.



*Figure 2. Examples of Streets with Different PCIs*

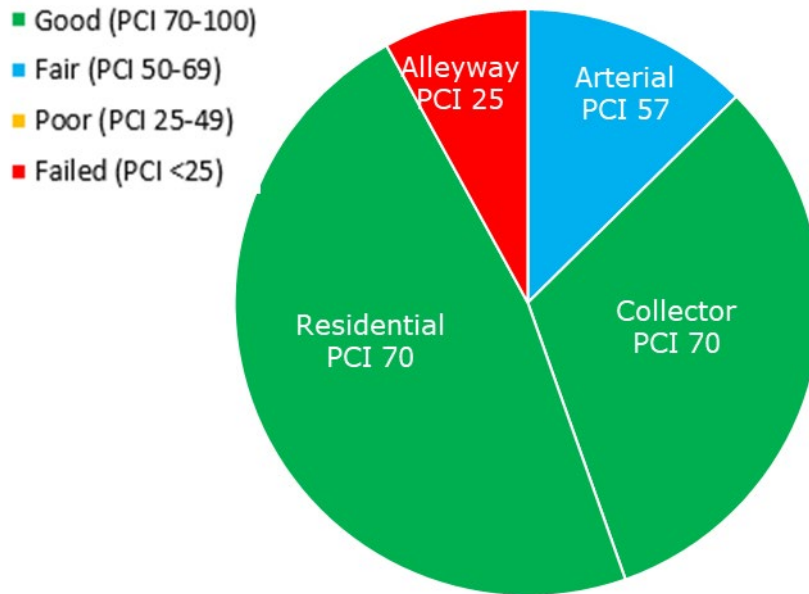
A list of all pavement sections in the network along with their attributes, including the PCI at the time of the last inspection, is provided in Appendix A. For convenience, two versions are provided – one sorted alphabetically by street name and the other sorted by descending PCI.

### 3.2 CITY'S NETWORK CONDITION BREAKDOWN

Figure 3 breaks down the current network PCI by functional classification. As shown, the residential and collectors have the highest condition with a PCI of 70 while the alleyways under 'other' functional class have the lowest condition (Failed) with a PCI

of 25. The area-weighted average PCI for arterials is 57 which represents “Fair” condition. The collectors and residential have similar PCI of 70 which represents the “Good” condition category. The average overall PCI for the City’s entire network is in the “Fair” condition category (65). The alleyways are in “Failed” condition with a PCI of 25.

Table 2 summarizes the pavement network by condition category and functional classification. Approximately 55.7 percent of the network are in “Good” condition, with approximately 19.1 percent in “Fair” condition. The remaining 25.2 percent are in “Poor” or “Failed” condition.



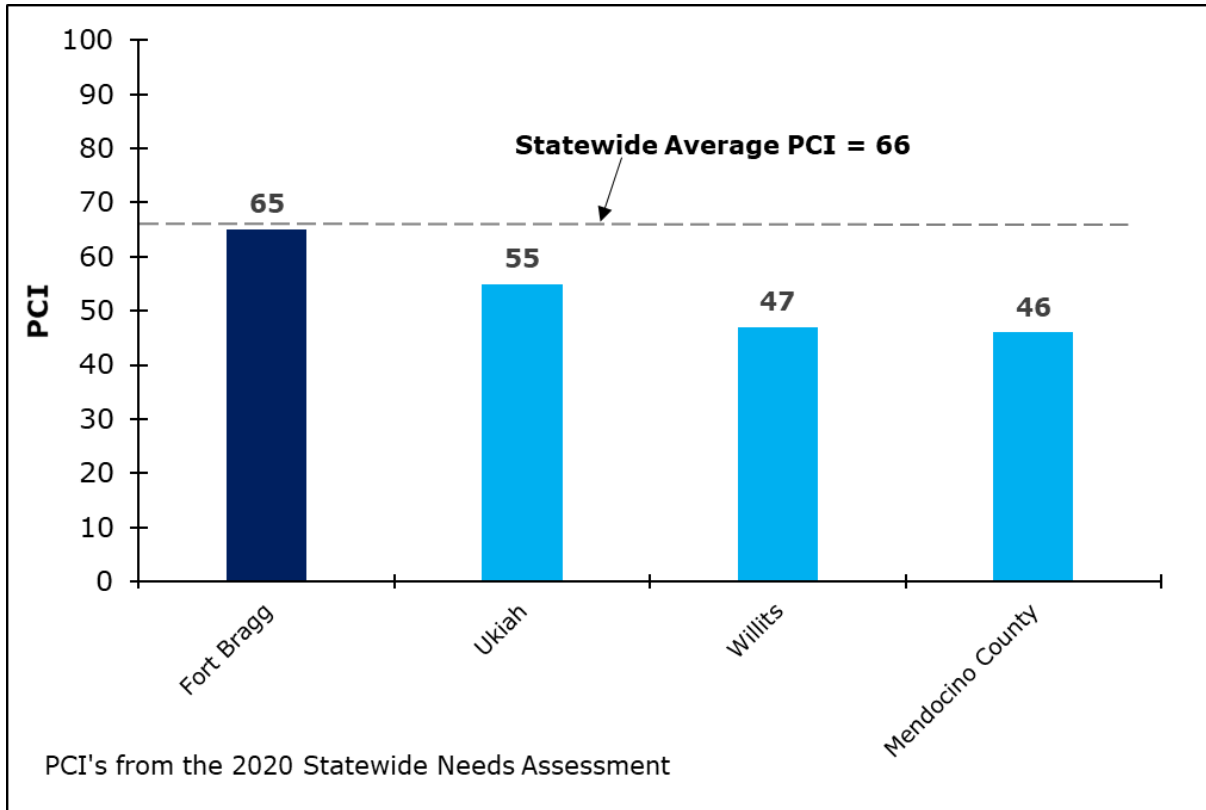
*Figure 3. Network Condition Breakdown by Functional Classification*

*Table 2. Pavement Condition Breakdown by Functional Class*

Condition Category	PCI Range	Arterial (%)	Collector (%)	Residential (%)	Alleyway (%)	Entire Network (%)
<b>Good</b>	70-100	2.9	20.5	30.7	1.6	55.7
<b>Fair</b>	50-69	3.7	8.2	7.2	0.0	19.1
<b>Poor</b>	25-49	5.5	0.9	4.0	0.4	10.8
<b>Failed</b>	<25	0.5	2.4	5.5	6.0	14.4
<b>Total</b>	-	<b>12.6</b>	<b>32.0</b>	<b>47.4</b>	<b>8.0</b>	<b>100.0</b>

**3.3 PCI COMPARISON WITHIN MCOG AGENCIES**

As a comparison, Figure 4 shows the City’s network PCI compared to the agencies in Mendocino County as well as the statewide average PCI. This comparison data was presented as part of the Statewide Needs Assessment<sup>3</sup>. As illustrated, the City’s network PCI is the highest within MCOG but still 1 points below the statewide average.



**Figure 4. Comparison of Network PCI to Statewide Average**

<sup>3</sup> California Statewide Local Streets and Roads Needs Assessment, Nichols Consulting Engineers Chtd., August 2021.

## 4 Maintenance and Rehabilitation Strategies

Historically, the City has frequently used crack seals, slurry seal, multilayer surface seals and HMA overlays as maintenance and rehabilitation strategies. In general chip seals/slurry seals will be applied to pavements in “Good” condition; multilayer rubberized cape/chip seal or a thin hot mix asphalt (HMA) overlay will be performed on pavements in “Fair” condition; mill with thick or thin HMA overlays will be performed on pavements in “Poor” condition; and reconstruction (FDR with HMA overlay) will be performed when pavements are in “Failed” condition. These M&R strategies were formalized into a decision tree<sup>4</sup> (presented in Appendix B), which is instrumental in performing the budget needs analysis and budget scenarios.

Experience and research have shown that it costs much less to maintain pavement in good condition than to repair pavements that have already failed. As shown in Figure 5, by allowing pavements to deteriorate, street that once cost \$5.5/square yard (SY) to seal may soon cost \$89/SY to overlay, or \$114.25/SY to reconstruct. In other words, delaying repairs can significantly increase M&R costs. Note that chip seals can be placed on approximately 21 times as many lane miles as those requiring surface reconstruction.

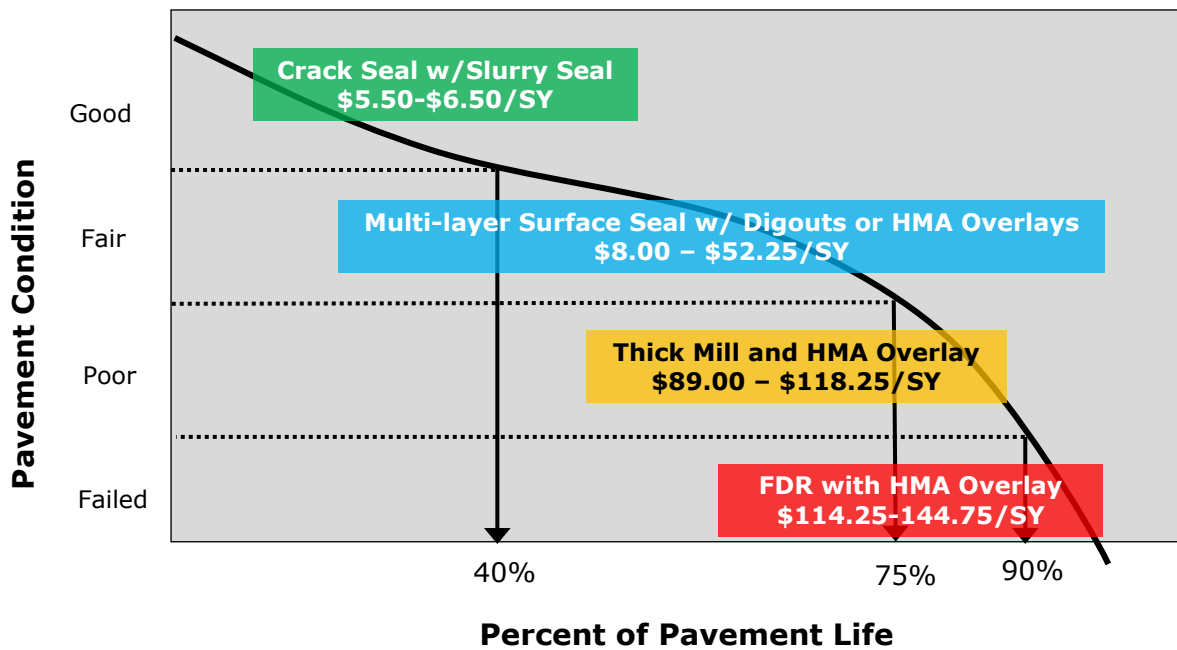


Figure 5. Costs of Maintaining Pavements Over Time

<sup>4</sup> Note: The StreetSaver® “Maintenance and Rehabilitation Decision Tree” divides the “Fair” condition category to separate pavements with primarily non-load-related distresses (e.g., longitudinal cracking) from those with load-related distresses (e.g., fatigue cracking).

## 5 Budget Analyses

Based on the principle that it costs less to maintain streets in good condition than it does to repair those that have failed, cost-effective PMPs employ strategies that eliminate the deferred maintenance<sup>5</sup> and then maintain the network with on-going preventive maintenance. Such strategies bring the network condition to an optimal PCI that can be maintained over time.

The first step in developing such a cost-effective strategy is to determine the total maintenance budget needs of the network. The next step is to conduct alternative budget scenario analyses. In consultation with the City, two funding scenarios were selected for analysis and performed using StreetSaver®:

- **Scenario 1: Existing Budget** – This scenario assumes the City will spend approximately \$1 million per year on pavement M&R for the next ten years.
- **Scenario 2: Improve PCI to 75**– This scenario aims to improve the existing network PCI of 65 to 75 by 2031.

The budget needs analysis and budget scenarios are presented in the following sections. The detailed results of the budget needs analysis are provided in Appendix C. The detailed results of the budget scenarios are provided in Appendix D. Additionally, maps illustrating the current pavement condition and the projected 2031 pavement condition for each scenario are provided in Appendix E.

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<sup>5</sup> Deferred maintenance is M&R not performed due to insufficient funding.

## 5.1 BUDGET NEEDS ANALYSIS

The total budget needs for the network represents the cost associated with performing M&R treatments at the optimal time – optimal meaning the PCI is maximized and the cost is minimized – over the analysis period. This was done by performing a budget needs analysis in StreetSaver® with an inflation rate of 3 percent for an analysis period of ten years.

The results of the budget needs analysis are presented in Table 3 and Table 4. Table 3 represents the budget needs for the entire network including alleyways. Table 4 represent the budget needs only for the alleyways. The total budget needs for the City for the next ten years is estimated to be \$25.3 million (Table 3). As can be seen from the Tables 3 and 4, the alleyways would need six percent of the total budget needs. Of the total budget needs for the entire network, approximately \$5.3 million (21 percent) is devoted to preventive maintenance, while the rest is allocated for more costly rehabilitation and reconstruction treatments.

**Table 3. Summary Results for Budget Needs Analysis (Entire Network)**

Year	2022	2023	2024	2025	2026	2027	2028	2029	2030	2031	Total
Total Budget Needs (\$M)	19.55	0.26	0.88	0.00	0.03	1.98	0.07	2.02	0.20	0.33	25.32
Treated PCI	92	88	87	85	84	86	84	85	85	84	NA
Untreated PCI	65	62	60	58	56	54	51	49	47	45	NA

**Table 4. Summary Results for Budget Needs Analysis (Alleyways)**

Year	2022	2023	2024	2025	2026	2027	2028	2029	2030	2031	Total
Budget Needs (\$M) Alleyways	1.39	0.00	0.00	0.00	0.00	0.00	0.00	0.01	0.11	0.00	1.51
Treated PCI (Alleyways)	99	93	91	89	88	87	85	84	90	88	NA
Untreated PCI (Alleyways)	25	23	22	20	20	19	18	17	17	17	NA

If the City follows this ideal, cost-effective strategy, the average network PCI will immediately increase as a large amount of deferred maintenance is addressed in the first year, and then stabilize around mid-80s. This type of budget, that addresses the current deferred maintenance in the first year, is known as front-loaded. Alternatively, if no maintenance is performed over the next ten years, the PCI will drop to 45. The detailed results of the budget needs analysis are provided in Appendix C.



## 5.2 SCENARIO 1: EXISTING FUNDING (\$10.0 M/10 YEARS)

This scenario assumes the City will have \$1 million to dedicate to pavement M&R each year for the next ten years. As shown in Table 5 and Figure 6, the overall network PCI will be maintained between high-60s and low-70s for next ten years. The alleyways will be in "Fair" condition with a PCI of 60 by the end of the analysis period. While Table 5 represents budget allocation and the projected PCI for the entire network, Table 6 and Table 7 show the projected budget allocation and project PCI individually for streets and alleyways, respectively. More than half of the network (60.8 percent) will be in "Good" condition with 20.7 percent in "Failed" condition. The deferred maintenance will increase to \$30 million by 2031. A list of sections selected for treatment are provided in Appendix F.

**Table 5. Summary Results for Scenario 1 (Entire Network)**

Year	2021 before work	2022	2023	2024	2025	2026	2027	2028	2029	2030	2031	Total
Total Budget (\$M)	NA	1.0*	1.0	1.0	1.0	0.9	0.9	1.0	0.6	0.9	0.9	9.2
Treated PCI	65	66	65	64	64	63	62	61	60	59	58	NA
Deferred Maintenance (\$M)	20	18	19	20	20	23	26	26	28	29	30	NA

\*Includes planned project in 2022

**Table 6. Summary Results for Scenario 1 (Streets)**

Year	2021 before work	2022	2023	2024	2025	2026	2027	2028	2029	2030	2031	Total
Budget (\$M) Streets	NA	1.0	0.9	1.0	0.9	0.9	0.9	0.9	0.6	0.9	0.9	8.9
Treated PCI Streets	68	70	69	68	67	66	64	63	62	61	60	NA

**Table 7. Summary Results for Scenario 1 (Alleyways)**

Year	2021 before work	2022	2023	2024	2025	2026	2027	2028	2029	2030	2031	Total
Budget (\$M) Alleyways	NA	0.00	0.10	0.00	0.10	0.00	0.00	0.10	0.00	0.00	0.00	0.3
Treated PCI Alleyways	25	25	26	26	28	29	29	32	33	34	35	NA

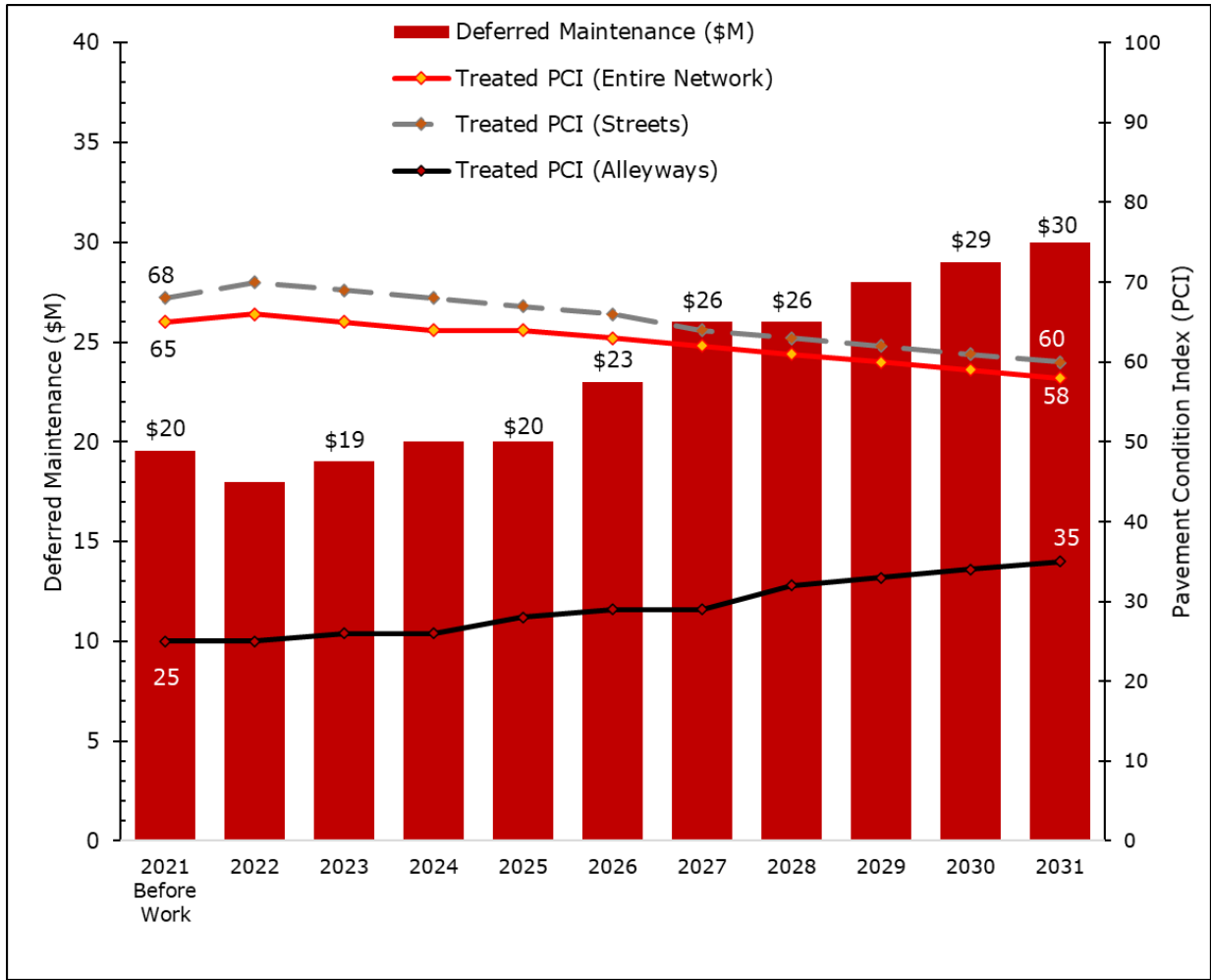


Figure 6. PCI vs Deferred Maintenance for Scenario 1

**5.3 SCENARIO 2: IMPROVE PCI TO 75 (\$26.7 M/10 YEARS)**

This scenario aims to improve the network PCI from 64 to 75 by the end of the analysis period. As shown in Table 8 and Figure 7, the financial commitment required to accomplish this goal is \$26.7 million over ten years. While Table 8 represents budget allocation and the projected PCI for the entire network, Table 9 and Table 10 show the projected budget allocation and project PCI individually for streets and alleyways, respectively. Both streets and alleyways would be in “Good” condition with a PCI of 74 and 86, respectively. This will result in 84.5 percent of the entire network being “Good” condition with 11 percent, in “Failed” condition. The deferred maintenance will decrease to \$11 million by the end of analysis period.

**Table 8. Summary Results for Scenario 2 (Entire Network)**

Year	2021 before work	2022	2023	2024	2025	2026	2027	2028	2029	2030	2031	Total
Total Budget (\$M)	NA	0.9*	1.3	0.8	1.1	3.4	4.7	4.1	3.4	4.0	3.0	26.7
Treated PCI	65	66	68	69	68	69	70	72	72	74	75	NA
Deferred Maintenance (\$M)	20	18	18	19	20	20	18	15	14	11	11	NA

\*Includes planned project in 2022

**Table 9. Summary Results for Scenario 2 (Streets)**

Year	2021 before work	2022	2023	2024	2025	2026	2027	2028	2029	2030	2031	Total
Budget (\$M) Streets	NA	0.90	0.70	0.10	1.10	3.40	4.70	4.10	3.40	4.00	3.00	25.4
Treated PCI Streets	68	70	69	67	66	67	69	70	71	73	74	NA

**Table 10. Summary Results for Scenario 2 (Alleyways)**

Year	2021 before work	2022	2023	2024	2025	2026	2027	2028	2029	2030	2031	Total
Budget (\$M) Alleyways	NA	0.00	0.60	0.70	0.00	0.00	0.00	0.00	0.00	0.00	0.00	1.3
Treated PCI Alleyways	25	27	58	93	90	89	89	87	87	85	86	NA

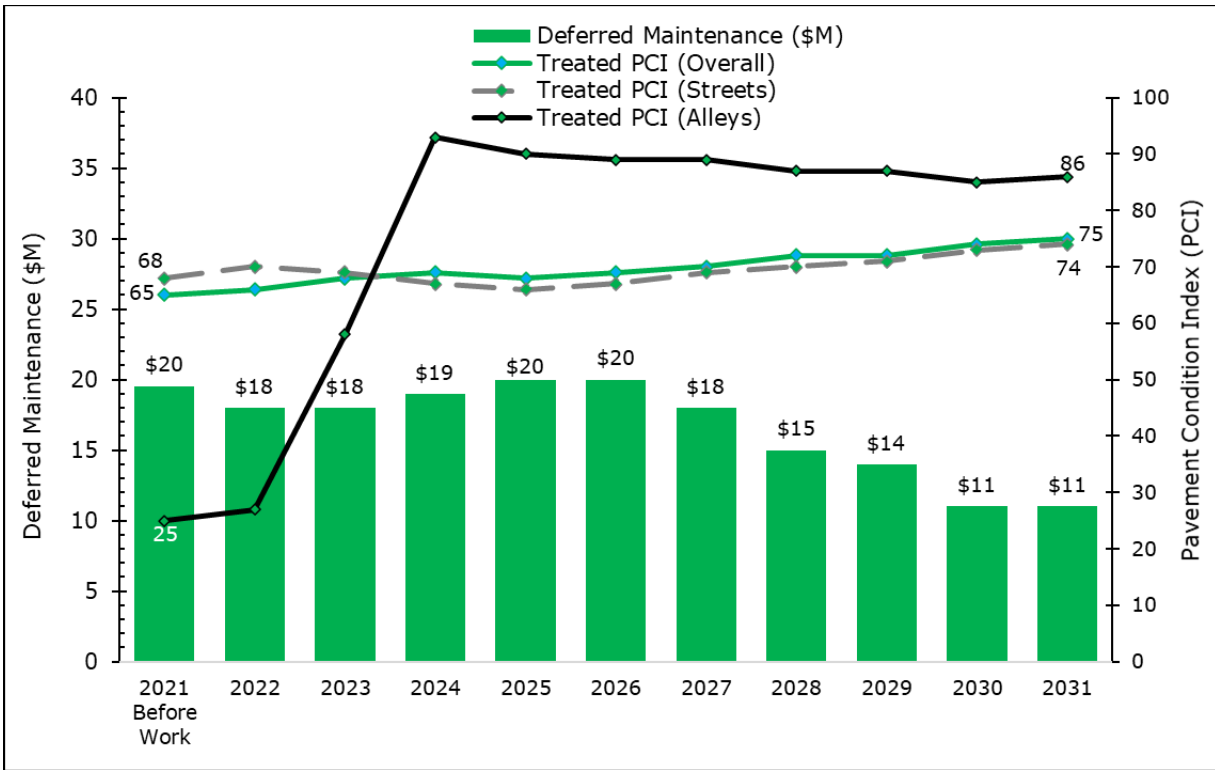


Figure 7. PCI vs Deferred Maintenance for Scenario 2

5.4 SCENARIO COMPARISONS

Figure 8 graphically compares the annual changes in PCI for each of the scenarios. As previously noted, the PCI will be maintained between high-50s and mid 60s in Scenario 1 and will be improved to 75 by 2031 in Scenario 2.

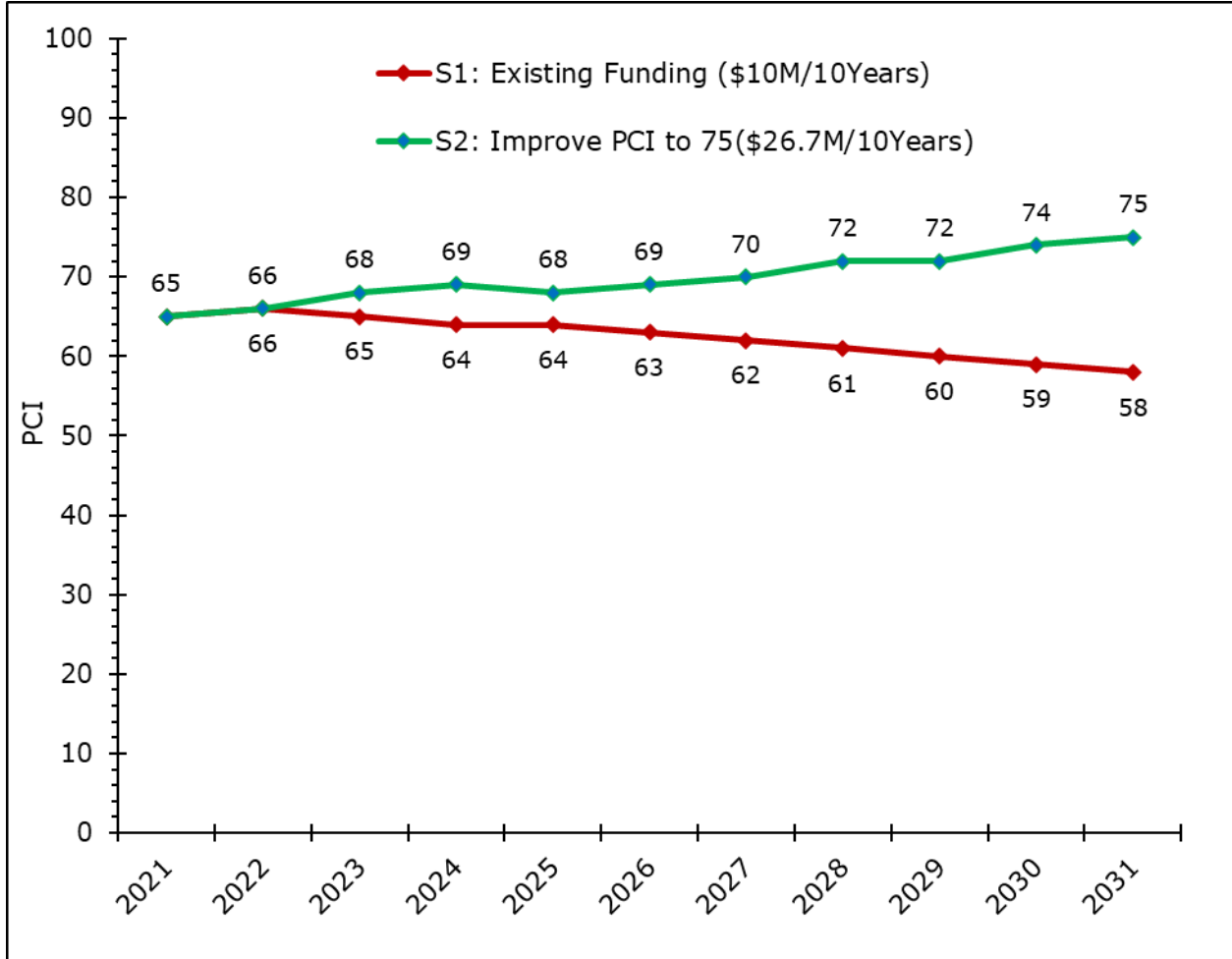


Figure 8. Comparison of Annual PCI by Scenario

Figure 9 illustrates the changes in deferred maintenance over time for each scenario. For Scenario 1, the deferred maintenance will slightly increase to \$30 million by the end of analysis period. In Scenarios 2 the deferred maintenance will be almost half by the end of 2031.

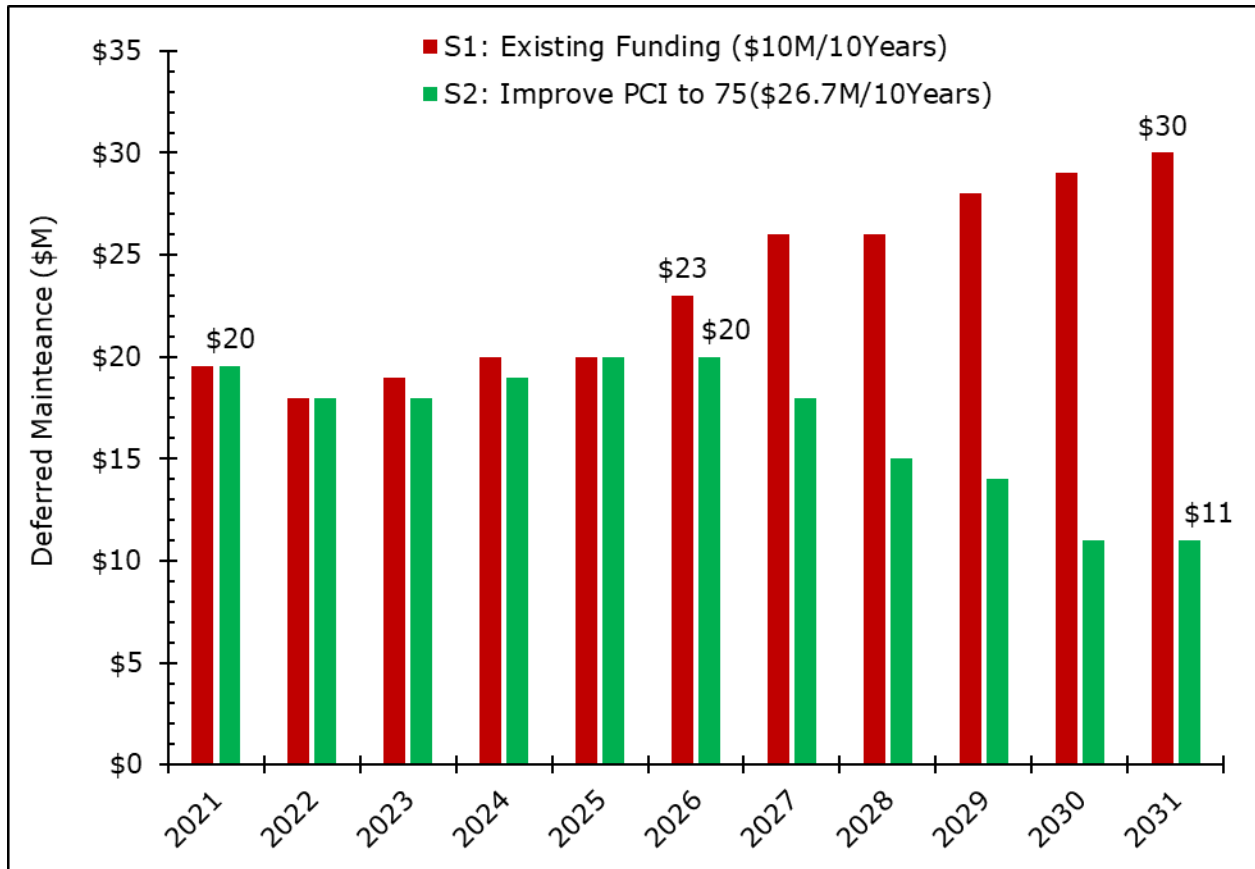


Figure 9. Comparison of Annual Deferred Maintenance by Scenario

Figure 10 illustrates the percent change in pavement condition for each scenario by the end of the analysis period. As noted earlier, currently, 55.7 percent of the network is in “Good” condition, with 25.2 percent in “Poor” or “Failed” condition. Both scenarios will increase the portion of the network in “Good” condition. The portion in “Failed” condition will increase and decrease in Scenarios 1 and 2, respectively. More than half of the entire pavement network (60.8 percent) will be in “Good” condition with 14.4 percent in ‘Failed” condition under Scenario 1. For Scenario 2, the portion of the network in “Good” condition will increase to 84.5 percent and the portion of the network in “Poor” condition will be eliminated.

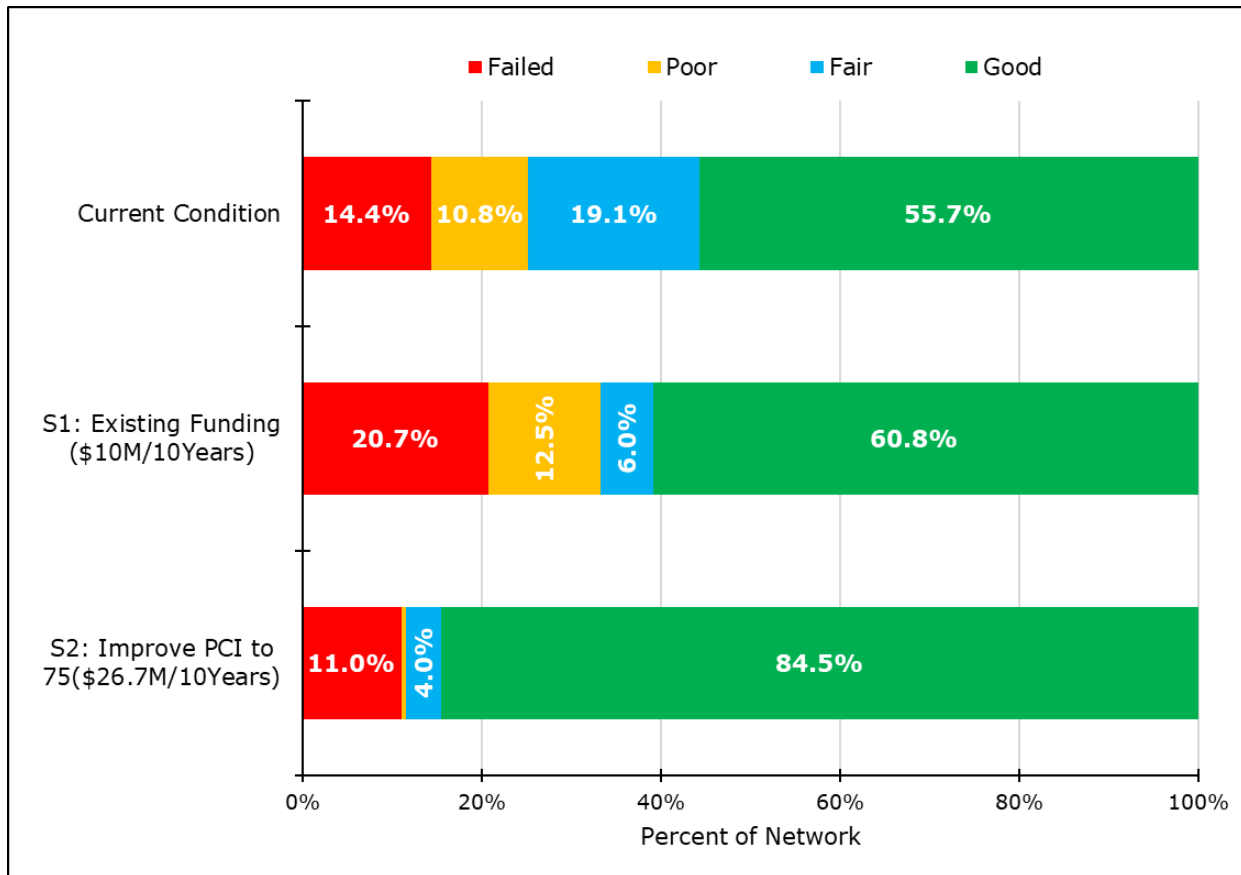


Figure 10. Comparison of Pavement Condition Breakdown by Scenario

## 6 Conclusion and Recommendations

In summary, City of Fort Bragg has a substantial investment of \$85.4 million in the pavement network. Overall, the City's streets are in "Fair" condition with a 2021 PCI of 65 which includes the alleyways. Approximately 55.7 percent of the street network is in "Good" condition and 14.4 percent is in "Failed" condition.

The analyses indicate that the City needs to spend approximately \$25.3 million on maintenance and rehabilitation over the next ten years to optimally repair all pavement sections, thus bringing the network into a condition that can be maintained with on-going preventive maintenance. In the long run, this strategy will save the City money by preventing future pavement deterioration to levels requiring rehabilitation or reconstruction.

Based on the data collected and the scenarios analyzed and presented in this report, NCE offers the following recommendations.

- 1. Funding** - The primary goal of PMPs should be to offer users a safe and functional pavement network without unduly increasing the maintenance burden in the future. With that in mind, the recommended scenario for the City is Scenario 2, which requires \$26.7 million over the next ten years to improve the PCI to 75 by the end of analysis period. This budget allocation will increase the portion of the network in "Good" condition and limit the increase in deferred maintenance.

To address the gap between the City's existing funding and the recommended scenario, NCE recommends the City pursue additional funding sources. Potential sources include:

### Federal Funding Sources

- Regional Surface Transportation Program (RSTP)
- Surface Transportation Program (STP)
- Congestion Mitigation and Air Quality Improvement Program (CMAQ)
- Community Development Block Grants (CDBG)
- Highway Safety Improvement Program (HSIP)
- Federal Emergency Management Agency (FEMA)

### State Funding Sources

- Active Transportation Program (ATP), which now includes the Bicycle Transportation Account (BTA) and Safe Routes to Schools (SR2S)
- State Transportation Improvement Program (STIP)
- Vehicle License Fees (VLF)
- CalRecycle grants
- State Water Resource Control Board



- Transportation Development Act (TDA)
- Traffic Safety Fund
- Transportation Uniform Mitigation Fee (TUMF)

#### Local/Regional Funding Sources

- Sales tax measure
- Development impact fees
- General funds
- Various assessment districts (lighting, maintenance, flood control, community facilities)
- Traffic impact fees
- Utilities (e.g., stormwater, water, wastewater enterprise funds)
- Parcel/property taxes
- Vehicle registration fees
- Vehicle code fines

2. **Pavement Management Strategies** – Since more than half of City’s streets are currently in “Good” condition, it is important to maintain that condition to the extent possible. Preservation occurs when streets with PCIs higher than 70 receive treatments such as surface seals (slurry, chip, microsurfacing, etc.). Seals are relatively inexpensive treatments that prevent moisture ingress and thus preserve the integrity of the underlying base material. NCE recommends that the City balance preventive maintenance with rehabilitation and reconstruction projects to preserve pavements in “Good” or “Fair” condition, improve pavements in “Poor” condition, and avoid increasing the deferred maintenance.
3. **Reinspection Strategies** – In order to make appropriate management decisions based on current data, NCE recommends that the City perform condition inspections on arterials and collectors every 2 years and on residential at least every 4 to 5 years. Additionally, since StreetSaver® and other prediction models do not yet consider the effect of specialized materials such as asphalt-binders with rubber or polymers, the actual performance of City pavements may not be fully modeled in the analysis. For this additional reason, NCE recommends regular pavement condition surveys to ensure model accuracy and relevance.
4. **M&R Decision Tree** – NCE recommends that the City annually review and Fwicurrent construction techniques and changing costs. This will ensure that the results for the budget analyses are reliable and as accurate as possible.

## Appendix A

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### SECTION DESCRIPTION INVENTORY

## Section Description Inventory Report

This report lists a variety of section description information for each of the agency's pavement sections. It lists the street and section identifiers, limits, functional class, surface type, number of lanes, length, width, area, Inspected PCI, and PCI date.

All of the agency's pavement sections are included in the report. Two versions of the report are provided. The first is sorted alphabetically by Street Name and Section ID and the second report is sorted by descending PCI. The field descriptions in this report are listed below:

<b>COLUMN</b>	<b>DESCRIPTION</b>
Street ID	Street Identification - A code up to ten characters/digits to identify the street. Generally, the street name is truncated to six characters. The Street ID should be unique for each street.
Section ID	Section Identification - A code up to ten characters/digits to identify the section number. The Section ID must be unique for each section of one street.
Street Name	Street Name - The name of the street as indicated by signs in the field.
Begin Location	Beginning limit of the section.
End Location	Ending limit of the section.
No. of Lanes	Number of travel lanes.
Functional Class (FC)	Functional Classification: Arterial; Collector; Residential; Other
Length (ft)	Length of the section in feet.
Width (ft)	Average width of the section in feet.
Area (sf)	Area of section in square feet.
Surface Type (ST)	Surface Type: AC (Asphalt Concrete), ST (Surface Treated), Gravel
Last M&R Date	The date of last maintenance or rehabilitation
Last M&R Treatment	Type of treatment (maintenance or rehabilitation) received the last time
PCI Date	The last inspection date or treatment date (whichever is the latest)
PCI	Average PCI for the section. The value is based on the last inspection or last treatment (whichever is the latest).

## **Section Description Inventory – Sorted by Street Name**

# **STREETS**

City of Fort Brag  
PCI List by Street Name  
2021 PMP Update

Street ID	Section ID	Street Name	Beg Location	End Location	Lanes	Functional Class	Length	Width	Area	Surface Type	Last M&R Date	Last M&R Treatment	Inspection Date	PCI
AIRPOR	001A	AIRPORT ROAD	C.L. SIGN 0+00	W/C.L. SIGN 5+87	2	Residential/Local	587	22	12,914	AC			6/12/2021	97
AIRPOR	001B	AIRPORT ROAD	W/C.L. SIGN 5+87	E.EDGE HWY1 @ LIMIT LINE 8+55	2	Residential/Local	268	22	5,896	AC			6/12/2021	97
ALGER	001	ALGER ST.	N.F.CURB LAUREL 0+00	END OF ST. 5+64	2	Residential/Local	564	22	12,408	AC/AC	7/1/2019	Mill and Overlay	6/12/2021	100
AZALEA	001	AZALEA CIRCLE	W.F. CURB S. SANDERSON 0+00	END OF CULDESAC	2	Residential/Local	275	38	10,450	AC			6/10/2021	17
BOATYA	001	BOATYARD DR.	5+30 FORM HWY1 0+00	4+32 FROM BEGIN	2	Arterial	432	24	10,368	AC/AC	7/1/2017	Mill and Overlay	6/10/2021	99
BOATYA	002	BOATYARD DR.	N.SIDE DRIVE WY HARVEST 0+00	N.F. HWY20 7+85	2	Arterial	785	41	32,185	AC/AC	7/1/2017	Mill and Overlay	6/10/2021	50
BRANDO	001	BRANDON WAY	N.F. CURB E. FIR ST. 0+00	GUARD BARRIER @ WINIFRED 7+28	2	Residential/Local	728	34	24,752	AC			6/10/2021	85
CEDAR	001	CEDAR ST. 001	E.F. CURB HAROLD ST. 0+00	END OF CURB 1320 CEDAR 22+75	2	Residential/Local	2,275	27	61,425	AC	1/1/2009	SLURRY SEAL	6/10/2021	59
CEDAR	002	CEDAR ST. 002	END CURB 1320 CEDAR 0+00	1631 EAST PROP. LINE 8+90	2	Residential/Local	350	19	6,650	AC	1/1/2009	SLURRY SEAL	6/10/2021	44
CHESTN	001	CHESTNUT ST.	E.F. CURB HWY1 0+00	END OF CURB-GUTTER S. SIDEWALK	3	Arterial	498	40	19,920	AC			6/9/2021	8
CHESTN	005	CHESTNUT ST.	END CURB-GUTTER@001,0+00	S. WHIPPLE	2	Collector	884	32	28,288	AC/AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/9/2021	84
CHESTN	010	CHESTNUT ST.	S. WHIPPLE	SANDERSON WY	2	Collector	2,937	32	93,984	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/10/2021	68
CHESTN	015	CHESTNUT ST.	SANDERSON WY	END OF STREET (9+75)	2	Collector	975	32	31,200	AC/AC	1/1/2009	AC OVERLAY 1.5 INCHES	6/9/2021	74
CELERI	001	CHIEF CELERI DR	W OAK ST	W ALDER ST	2	Arterial	475	15	7,125	AC			6/10/2021	7
DELM	001	DELMAR DR	OCEAN VIEW DR	END NORTH	2	Residential/Local	755	21	15,855	ST			6/10/2021	84
DELM	002	DELMAR DR	OCEAN VIEW DR	END SOUTH	2	Residential/Local	242	30	7,260	AC			6/10/2021	89
DENNIS	001	DENNISON LANE	S.EDGE OF PAVEMANT 0+00	N.W.PROP LN. STANLEY 8+53	2	Residential/Local	853	14	11,942	AC			6/15/2021	47
DICKWI	001	DICKWILLIAMS WAY	DANA ST	END AT CDS	2	Residential/Local	185	28	5,180	AC			6/15/2021	11
DUBOIS	001	DUBOIS LANE	S.F. CURB WALNUT 0+00	END OF PAVEMENT 2+07	2	Residential/Local	207	25	5,175	AC/AC	7/1/2019	Mill and Overlay	6/9/2021	100
EALDER	001	EAST ALDER 001	E.F CURB HWY 1 0+00	W.F.CURB N.FRANKLIN ST. 3+40	1	Collector	340	43	14,620	AC	1/1/2009	RECONSTRUCT SURFACE (AC)	6/10/2021	84
EALDER	002A	EAST ALDER 002	E.F.CURB S.FRANKLIN 0+00	HAROLD	1	Collector	1,584	43	68,112	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/10/2021	75
EALDER	002B	EAST ALDER 002	HAROLD	@HYDRANT END OF ST. 28+45	1	Residential/Local	1,261	43	54,223	AC	1/1/2009	RUBB CHIP SEAL/SLURRY	6/10/2021	82
EBUSH	001	EAST BUSH ST.	W.F. CURB BRANDON WAY 0+00	ES.ALLEY PRKINS&HARISON 12+23	2	Residential/Local	471	33	15,543	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/10/2021	95
EBUSH	005	EAST BUSH ST.	ES.ALLEY PRKINSHARISON 0+00	N. FRANKLIN ST	2	Residential/Local	890	43	38,270	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/10/2021	89
EBUSH	010	EAST BUSH ST.	N. FRANKLIN ST	E.F. CURB HWY1 12+23	2	Residential/Local	345	43	14,835	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/10/2021	93
ECYPRES	001	EAST CYPRESS ST. 001	END OF KEMPEE @ VALVE BOX 0+0	W.F.CURB RIVER DRV 12+96	2	Collector	1,296	40	51,840	AC/AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	82
ECYPRES	002	EAST CYPRESS ST. 002	W.F.CURB RIVER DRV.0+00	E.SIDE G.P. HAUL RD. 8+13	2	Collector	813	40	32,520	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	21
ECYPRES	003	EAST CYPRESS ST. 003	E.SIDE G.P. HAUL RD.0+00	E.F. HWY1 6+80	2	Collector	680	43	29,240	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	54
EELM	001	EAST ELM ST.	E.F.@ HWY1 0+00	W.F. CURB N FRANKLIN ST 3+25	2	Collector	325	44	14,300	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/11/2021	77
EFIR	001	EAST FIR ST. 001	E.F.CURB @ N FRANKLIN ST	E.F.CURB HAROLD 17+45	2	Collector	1,745	43	75,035	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/11/2021	78
EFIR	002	EAST FIR ST. 002	E.F.CURB @ HWY 1 0+00	RR TRACKS	2	Collector	142	44	6,248	AC	1/1/2009	RECONSTRUCT SURFACE (AC)	6/11/2021	36
EFIR	003	EAST FIR ST. 002	RR TRACKS	W.F.CURB H FRANKLIN	2	Collector	192	44	8,448	AC/AC	7/1/2019	Mill and Overlay	6/11/2021	100
ELAURE	002	EAST LAUREL 002	E.F.CURB N.HARRISON 0+00	W.F.CURB N.HAROLD 10+05	2	Collector	1,005	40	40,200	AC/AC	1/1/2009	AC OVERLAY 1.5 INCHES	6/12/2021	76
ELAURE	001	EAST LAUREL ST. 001	E.F. CURB HWY1 0+00	W.F. CURB HARRISON ST 9+25	2	Collector	925	45	41,625	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/12/2021	88
ELAURE	005	EAST LAUREL ST.003	W.F. CURB N. HAROLD 0+00	MORROW ST	2	Residential/Local	412	40	16,480	AC			6/12/2021	91
ELAURE	010	EAST LAUREL ST.003	MORROW ST	W.F. CURB END ST. @PARK 8+30	2	Residential/Local	413	40	16,520	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/12/2021	90
OAK	001	EAST OAK ST. 001	E.F. CURB HWY 1 0+00	W.F.CURB MCKINLEY 32+25	2	Arterial	3,225	45	145,125	AC	1/1/2009	SLURRY SEAL	6/9/2021	82
OAK	002	EAST OAK ST. 002	E.F. CURB MCKINLEY ST. 0+00	BACK E.SIDE SDWLK. DANA 17+25	2	Residential/Local	1,725	40	69,000	AC	1/1/2009	SLURRY SEAL	6/10/2021	78
OAK	003	EAST OAK ST. 003	BACK E.SIDE SDWLK DANA 0+00	CITY LIMIT SIGN OAK 16+10	2	Residential/Local	1,610	24	38,640	AC	1/1/2009	SLURRY SEAL	6/10/2021	62

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EPINE	005	EAST PINE ST.	N.F. CURB HWY1 0+00	CORRY ST	2	Collector	1,574	43	67,682	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/15/2021	53
EPINE	010	EAST PINE ST.	CORRY ST	W.F. CURB HAROLD ST 19+30	2	Collector	356	43	15,308	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/15/2021	80
ESPRUC	001	EAST SPRUCE ST.	E.F. CURB HWY1 0+00	CEMETARY GATE 5+07	2	Residential/Local	507	45	22,815	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/11/2021	50
EBBING	001	EBBING WAY	S.F. CURB CHESTNUT 0+00	END OF CULDESAC 5+62	2	Residential/Local	562	37	20,794	AC			6/9/2021	95
EDDY	001	EDDY ST.	W.F. CURB S SANDERSON 0+00	END OF STREET 3+18	2	Residential/Local	318	41	13,038	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/9/2021	83
ESPEY	001	ESPEY WAY	S.F. CURB WALNUT ST 0+00	END OF PAVEMENT 2+58	2	Residential/Local	258	36	9,288	AC/AC	7/1/2019	Mill and Overlay	6/9/2021	100
FERN	001	FERN AVE.	E.F. CURB MORROW ST 0+00	E.F. CURB END OF ST. 3+22	2	Residential/Local	322	29	9,338	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/12/2021	95
FLOREN	001	FLORENCE ST.	S.F. CURB OAK 0+00	N.F. CURB WILLOW ST. 6+03	2	Residential/Local	603	31	18,693	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	83
FRONTA	001	FRONTAGE ROAD(UNNAMED)	N.END @ LIMIT LINE 0+00	N.F. CURB OCEAN VIEW. W 6+80	2	Residential/Local	680	24	16,320	AC			6/16/2021	60
GLASSB	001	GLASSBEACH DR	WEST ELM ST	END AT BRIDGE	2	Residential/Local	2,199	33	72,567	AC			6/11/2021	59
GROVE	001	GROVE ST.	N.F. CURB WALNUT 0+00	S.F. CURB CHESTNUT ST 6+25	2	Residential/Local	625	30	18,750	AC	1/1/2009	SLURRY SEAL	6/14/2021	95
HALSEY	001	HALSEY WAY	E.F. CURB MCKINLEY ST.0+00	E.F. CURB HALSY WAY 2+85	2	Residential/Local	285	32	9,120	AC/AC	7/1/2019	Mill and Overlay	6/12/2021	100
HARAV	001	HARBOR AVE	OCEAN VIEW DR	SOUTH END	2	Residential/Local	90	39	3,510	AC/AC			6/10/2021	50
HARAV	002	HARBOR AVE	OCEAN VIEW DR	NORTH END	2	Residential/Local	140	32	4,480	AC/AC			6/10/2021	34
HAZEL	001	HAZEL ST.	E.F CURB HWY1 0+00	W.F.CURB S.FRANKLIN 3+40	2	Residential/Local	340	32	10,880	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/10/2021	52
HAZEL	002	HAZEL ST.	E.F.CURB S. FRANKLIN 0+00	W.F.CURB S.McPHERSON 3+15	2	Residential/Local	315	32	10,080	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/10/2021	95
HAZEL	003	HAZEL ST.	E.F.CURB S. MCPHERSON 0+00	W.F.CURB S.HARRISON 2+95	2	Residential/Local	295	27	7,965	AC			6/10/2021	100
HAZEL	004	HAZEL ST.	S HARRISON ST	EST END	1	Residential/Local	155	12	1,860	AC			6/10/2021	7
HAZELW	001	HAZELWOOD ST.	S.F. SOUTH ST. 0+00	HYDRANT @ END OF ST.3+60	2	Residential/Local	360	28	10,080	AC			6/14/2021	65
HOCKER	001	HOCKER LANE	S.F.SIDEWALK OAK ST.0+00	END OF CULDESAC 6+26	2	Residential/Local	777	36	27,972	AC			6/12/2021	20
HOLMES	001	HOLMES LANE	E.F. CURB OLD HAUL ROAD 0+00	W.F. CURB STEWART ST. 5+60	2	Residential/Local	560	36	20,160	AC			6/11/2021	37
HOWLAN	001	HOWLAND CT.	N.F. CURB OAK ST.0+00	END OF CULDESAC 2+30	2	Residential/Local	230	36	8,280	AC			6/12/2021	30
JEWETT	001	JEWETT ST.	S.F.SIDEWALK OAK ST.0+00	END OF CULDESAC 5+36	2	Residential/Local	536	39	20,904	AC	1/1/2009	SLURRY SEAL	6/12/2021	73
JOHNCIMO	001	JOHN CIMOLINO WAY	STEWART ST	END	2	Residential/Local	286	36	10,296	AC			6/11/2021	50
KEMPPE	005	KEMPPE WAY	E.F. HOSPITAL WY 0+00	433 FT E/O HOSPITAL WY	2	Residential/Local	433	37	16,021	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	96
KEMPPE	010	KEMPPE WAY	433 FT E/O HOSPITAL WY	N. OF VALVE BOX 9+35	2	Residential/Local	502	37	18,574	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	60
LIVING	001	LIVINGSTON ST.	S.F.CURB OAK 0+00	N.F.CURB WILLOW ST.6+17	2	Residential/Local	617	35	21,595	AC/AC	1/1/2009	AC OVERLAY 1.5 INCHES	6/14/2021	73
LONNE	001	LONNE WAY	E.F. SNADERSON 0+00	END OF CULDESAC 4+87	2	Residential/Local	487	32	15,584	AC			6/9/2021	44
MADRON	001	MADRONE ST. 001	E.F.CURB @ HWY 1 0+00	W.F.CURB @ McPHERSON ST.6+50	2	Residential/Local	650	43	27,950	AC			6/10/2021	68
MADRON	005	MADRONE ST. 002	E.F. CURB @S MCPHERSON ST 0+00	CORRY ST	2	Residential/Local	900	44	39,600	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/10/2021	93
MADRON	010	MADRONE ST. 002	CORRY ST	W.F. CURB @ HAROLD 13+00	2	Residential/Local	400	44	17,600	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/10/2021	64
MANZAN	001	MANZANITA ST.	W.F. CURB N. FRANKLIN ST.0+00	E.F. CURB HWY1 3+30	2	Residential/Local	330	26	8,580	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/11/2021	70
MAPLE	001A	MAPLE ST. 001	W.F.CURB S.LINCOLN 0+00	S.HAROLD	2	Collector	640	45	28,800	AC			12/10/2021	100
MAPLE	001B	MAPLE ST. 001	S.HAROLD	ALLEY BETWN WHIPPLE HARRISON	2	Collector	1,000	45	45,000	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/10/2021	71
MAPLE	005A	MAPLE ST. 005	ALLEY BETWN WHIPPLE HARRISON	S. FRANKLIN	2	Collector	900	49	44,100	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/10/2021	70
MAPLE	005B	MAPLE ST. 005	S. FRANKLIN	E.F. CURB HWY 1	2	Collector	400	49	19,600	AC			6/10/2021	82
MCKINL	001	MCKINLEY ST.	N.F. CURB OAK ST. 0+00	S.F. CURB E.ALDER ST, 4+10	2	Residential/Local	410	32	13,120	AC			6/12/2021	6
MINNES	001	MINNESOTA AVE.	S.F. CURB CHESTNUT ST. 0+00	CITY LIMITS 6+30	2	Residential/Local	630	32	20,160	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/9/2021	92

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Street ID	Section ID	Street Name	Beg Location	End Location	Lanes	Functional Class	Length	Width	Area	Surface Type	Last M&R Date	Last M&R Treatment	Inspection Date	PCI
MORROW	001	MORROW ST. 001	N.F. CURB OAK ST.0+00	S.F. CURB CEDAR 6+54	2	Residential/Local	654	34	22,236	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/15/2021	95
MORROW	002	MORROW ST. 002	N.F. CURB CEDAR ST.0+00	S.F. CURB LAUREL 6+56	2	Residential/Local	656	42	27,552	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/15/2021	94
MYRTLE	001	MYRTLE ST.	S.F. N. HARBOR DR. 0+00	S.EDGE SOUTH ST. 5+55	2	Residential/Local	555	26	14,430	AC			6/11/2021	100
NANCY	001	NANCY WAY	N.F.CURB CEDAR 0+00	END OF PAVEMENT @ OSPREY 2+66	2	Residential/Local	266	31	8,246	AC			6/15/2021	55
NESS	001	NESS AVE.	E.F. CURB SANDERSON WAY 0+00	E. P.L. 111 NESS FENCE 2+45	2	Residential/Local	245	22	5,390	AC			6/12/2021	100
NCORRY	001	NORTH CORRY ST.	N.F.CURB OAK ST.0+00	S.F.CURB FIR ST. 22+47	2	Residential/Local	2,247	42	94,374	AC			6/10/2021	80
NDANA	001	NORTH DANA ST	EAST OAK ST	END	2	Residential/Local	319	28	8,932	AC			6/12/2021	61
NFRANK	001	NORTH FRANKLIN ST. 001	N.F.CURB @ E.ALDER 0+00	S.F.CURB @ E. FIR 17+77	2	Arterial	1,777	43	76,411	AC	6/1/2013	4" DIGOUTS	6/14/2021	63
NFRANK	002	NORTH FRANKLIN ST. 002	S.F.CURB @ E.FIR 0+00	N.F.CURB @ E. BUSH 5+35	2	Arterial	535	43	23,005	AC	6/1/2013	4" DIGOUTS	6/14/2021	51
NFRANK	003	NORTH FRANKLIN ST. 003	N.F.CURB @ E.BUSH 0+00	AC BURM @ END OF NORTH 13+51	2	Arterial	1,351	43	58,093	AC	6/1/2013	4" DIGOUTS	6/14/2021	35
NFRANK	004	NORTH FRANKLIN ST. 004	N.F.CURB @ OAK ST 0+00	N.F.CURB @ E.ALDER ST. 4+70	2	Arterial	470	43	20,210	AC	6/1/2013	4" DIGOUTS	6/14/2021	60
NHARBO	001	NORTH HARBOR DRIVE 001	CITY LIMITS @GUARD RAIL 0+00	E.F.CURB S. FRANKLIN 19+90	2	Residential/Local	1,990	28	55,720	AC			6/11/2021	73
NHARBO	002	NORTH HARBOR DRIVE 002	E.F.CURB S FRANKLIN ST. 0+00	E.F.CURB HWY 1 4+55	2	Residential/Local	455	32	14,560	AC			6/11/2021	82
NHAROL	001	NORTH HAROLD ST. 001	END OF PAVEMENT 0+00	N.S.DRVWY BEHIND M.SCHOOL3+16	2	Residential/Local	316	28	8,848	AC/AC	7/1/2019	Mill and Overlay	6/11/2021	100
NHAROL	002	NORTH HAROLD ST. 002	N.SIDE M.S.SCH.DRVWAY 0+00	N.CURB @ FIR ST.2+85	2	Residential/Local	285	40	11,400	AC/AC	7/1/2019	Mill and Overlay	6/11/2021	100
NHAROL	005	NORTH HAROLD ST. 005	S.F. CURB FIR ST 0+00	LAUREL ST	2	Collector	920	52	47,840	AC	8/1/2011	TRIPLE LAYER RUBBERIZED ASPHALT	6/11/2021	81
NHAROL	007	NORTH HAROLD ST. 005	ALDER ST	N.F. CURB OAK ST	2	Collector	400	52	20,800	AC	8/1/2011	MICROSURFACING/RUBBER CHIP SEAL/SLURRY	6/11/2021	60
NHAROL	006	NORTH HAROLD ST. 006	LAUREL ST	ALDER ST	2	Collector	920	52	47,840	AC/AC	8/1/2011	MICROSURFACING/RUBBER CHIP SEAL/SLURRY	6/11/2021	75
NHARRI	001A	NORTH HARRISON ST.	N.EDGE PAVEMENT @END ST0+00	REDWOOD AVE (EAST)	2	Collector	2,256	44	99,264	AC			6/15/2021	82
NHARRI	001B	NORTH HARRISON ST.	REDWOOD AVE (EAST)	N.F. CURB OAK ST	2	Collector	1,056	44	46,464	AC			6/15/2021	72
NMcPHE	001	NORTH McPHERSON ST.	N.F. CURB OAK ST. 0+00	S.F. CURB BUSH ST. 27+08	2	Collector	2,708	44	119,152	AC			6/15/2021	80
NSANDE	001	NORTH SANDERSON WAY 001	N.F. CURB OAK ST.0+00	S.F. CURB CEDAR 6+31	2	Residential/Local	631	29	18,299	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/12/2021	93
NSANDE	002	NORTH SANDERSON WAY 002	N.F. CURB CEDAR ST.0+00	S. P.L. 222 N SANDERSON 7+27	2	Residential/Local	727	20	14,540	AC/AC	7/1/2017	Mill and Overlay	6/12/2021	100
NWHIPP	001	NORTH WHIPPLE ST.	N.F.CURB OAK ST.0+00	ALDER ST	2	Residential/Local	450	44	19,800	AC/AC	7/1/2003	AC OVERLAY 2 INCHES	6/11/2021	40
NWHIPP	002	NORTH WHIPPLE ST.	ALDER ST	S.F.CURB FIR ST.	2	Residential/Local	1,850	44	81,400	AC			6/11/2021	85
NLINCO	001	NORTH LINCOLN ST.	N.F. CURB OAK ST. 0+00	S.F. CURB CEDAR ST. 6+52	2	Residential/Local	652	25	16,300	AC			6/12/2021	4
NOYOHI	001	NOYO HIEGHTS DRIVE	W.F. CURB SANDERSON 0+00	END OF CULDESAC 4+15	2	Residential/Local	415	36	14,940	AC			6/9/2021	36
NOYOPT	001	NOYO POINT RD	CYPRESS ST	END AT LOT	2	Residential/Local	1,828	24	43,872	AC			6/15/2021	98
OAKTER	001	OAK TERRACE COURT	N.F. CURB OAK ST. 0+00	END OF ST. 3+38	2	Residential/Local	338	36	12,168	AC	1/1/2009	SLURRY SEAL	6/12/2021	86
OCEANV	003	OCEAN VIEW DRV.EAST 003	E.F. CURB HWY 1 0+00	5+30 FROM HWY1	2	Residential/Local	530	26	13,780	AC			6/10/2021	83
OCEANV	001	OCEAN VIEW DRV.WEST 001	W.F. CURB HWY1 0+00	4+18 FROM W.F. HWY1	2	Residential/Local	418	36	15,048	AC/AC	8/1/2013	AC OVERLAY 2 INCHES	6/10/2021	69
OCEANV	002	OCEAN VIEW DRV.WEST 002	4+18 FROM HWY 1 0+00	C.L. 4+58	2	Residential/Local	458	28	12,824	AC/AC	8/1/2013	AC OVERLAY 2 INCHES	6/10/2021	10
OLSEN	001	OLSEN LANE	S.F. CURB CHESTNUT 0+00	END OF ST. 6+52	2	Residential/Local	652	28	18,256	AC/AC	7/1/2019	Mill and Overlay	6/9/2021	100
PARK	001	PARK ST. 001	S.F. CURB OAK 0+00	N.F.CURB WILLOW 6+00	1	Residential/Local	600	25	15,000	AC/AC	7/1/2003	AC OVERLAY 2 INCHES	6/14/2021	56
PARK	002	PARK ST. 002	S.F. CURB WILLOW 0+00	WHERE ST.NARROWS 300 BLK 5+68	2	Residential/Local	568	28	15,904	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	82
PARK	003	PARK ST. 003	END OF 002 0+00	END OF ST. 1+45	1	Residential/Local	145	14	2,030	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	67
PENITE	001	PENITENTI WAY	W.F. CURB S.SANDERSON 0+00	END OF CULDESAC 2+75	2	Residential/Local	275	38	10,450	AC			6/9/2021	18
PERKIN	001	PERKINS WAY	N.F. CURB E. FIR ST. 0+00	@ DEAD END10+53	2	Residential/Local	1,053	33	34,749	AC			6/11/2021	80
PUDDIN	001	PUDDING CREEK RD.	C.L SIGN0+00	E.F. HWY1 @ LIMIT LINE 9+55	2	Residential/Local	955	23	21,965	AC	6/1/2010	RECONSTRUCT SURFACE (AC)	6/11/2021	90
RASSMU	001	RASSMUSSEN LANE	N EDGE CEDAR 0+00	END OF GOOD PAVEMENT 4+55	2	Residential/Local	455	13	5,915	AC/AC	7/1/2019	Mill and Overlay	6/12/2021	100
EREDWO	005	REDWOOD AVE (EAST) 001	E.F. CURB HWY 1	N. HAROLD ST	2	Arterial	1,929	44	84,876	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/12/2021	71
EREDWO	010	REDWOOD AVE (EAST) 001	N. HAROLD ST	E.F. MORROW ST	2	Residential/Local	401	44	17,644	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/12/2021	93
RIVER	001	RIVER DRIVE	N.F. RIVER DR. 0+00	N. CURB HOSPITAL DR. 5+15	2	Residential/Local	515	44	22,660	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	95
SNUGH	001	SNUG HARBOR PLACE	OCEAN VIEW DR	SOUTH END	2	Residential/Local	110	19	2,090	AC			6/15/2021	96



City of Fort Brag  
 PCI List by Street Name  
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Street ID	Section ID	Street Name	Beg Location	End Location	Lanes	Functional Class	Length	Width	Area	Surface Type	Last M&R Date	Last M&R Treatment	Inspection Date	PCI
SCORRY	001	SOUTH CORRY ST. 001	N.F. CURB CHESTNUT ST. 0+00	S.F. CURB OAK ST.19+37	2	Residential/Local	1,937	38	73,606	AC			6/9/2021	95
SDANA	001	SOUTH DANA ST.	N.F. CUEB CHESTNUT ST 0+00	S.F. CURB OAK ST. 19+00	2	Collector	1,900	40	76,000	AC	8/1/2011	TRIPLE LAYER RUBBERIZED ASPHALT	6/10/2021	92
SFRANK	001	SOUTH FRANKLIN ST. 001	S.F. CURB OAK ST. 0+00	N.F. CURB CYPRESS 32+20	2	Arterial	3,220	43	138,460	AC/AC	7/1/2017	Mill and Overlay	6/14/2021	51
SFRANK	002	SOUTH FRANKLIN ST. 002	N.F.CURB @ CYPRESS 0+00	N.F.CURB @ SOUTH 8+63	2	Arterial	863	43	37,109	AC/AC	7/1/2017	Mill and Overlay	6/14/2021	44
SFRANK	003	SOUTH FRANKLIN ST. 003	N.F.CURB @ SOUTH 0+00	N.F.CURB @ S.HARBOR 5+65	2	Arterial	565	43	24,295	AC/AC	7/1/2019	Mill and Overlay	6/14/2021	61
SHAROL	001	SOUTH HAROLD ST. 001	S.F. CURB OAK ST. 0+00	N.F. CURB MAPLE ST 8+76	2	Collector	876	52	45,552	AC			6/14/2021	50
SHAROL	002	SOUTH HAROLD ST. 002	S.F. MAPLE ST. 0+00	N.F. CURB CHESTNUT ST. 9+75	2	Residential/Local	975	20	19,500	AC/AC	1/1/2009	AC OVERLAY 1.5 INCHES	6/14/2021	94
SHAROL	003	SOUTH HAROLD ST. 003	S.F. CHESTNUT ST 0+00	END OF ST.@W.POWER POLE 5+00	2	Residential/Local	620	16	9,920	AC			6/14/2021	14
SHARRI	001	SOUTH HARRISON ST.	N.F. CURB WALNUT ST. 0+00	S.F. CURB OAK ST. 25+45	2	Collector	2,545	44	111,980	AC	1/1/2009	SLURRY SEAL	6/15/2021	65
SLINCO	005	SOUTH LINCOLN ST.	N.F. CURB CHESTNUT ST	MAPLE ST	2	Collector	1,069	55	58,795	AC			6/14/2021	27
SLINCO	015A	SOUTH LINCOLN ST.	MAPLE ST	WILLOW ST	2	Collector	282	30	8,460	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	70
SLINCO	015B	SOUTH LINCOLN ST.	WILLOW ST	S.F. CURB OAK ST	2	Residential/Local	494	30	14,820	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	93
SMcPHE	001	SOUTH McPHERSON ST.	S.F. CURB OAK ST.	MADRONE	2	Collector	436	44	19,184	AC			6/15/2021	95
SMcPHE	002	SOUTH McPHERSON ST.	MADRONE	MAPLE	2	Collector	422	44	18,568	AC/AC	7/1/2003	AC OVERLAY 2 INCHES	6/15/2021	53
SMcPHE	003	SOUTH McPHERSON ST.	MAPLE	N.F. CURB CHESTNUT ST.	2	Collector	1,042	44	45,848	AC			6/15/2021	91
SSANDE	001	SOUTH SANDERSON ST. 001	N.F. CURB CHESTNUT ST 0+00	S.F. CURB OAK ST 19+15	2	Collector	1,915	40	76,600	AC			6/9/2021	92
SSANDE	005	SOUTH SANDERSON ST. 002	END OF W. SIDEWALK 0+00	412 FT N/O END OF W. SIDEWALK	2	Residential/Local	412	30	12,360	AC			6/9/2021	93
SSANDE	010	SOUTH SANDERSON ST. 002	412 FT N/O END OF W. SIDEWALK	S.F. CURB CHESTNUT ST 1040	2	Residential/Local	628	30	18,840	AC			6/9/2021	85
SOUTH	005	SOUTH ST. 005	E. EDGE VALLEY GUTTER HWY 1	HAZELWOOD ST	2	Collector	1,218	38	46,284	AC			6/14/2021	81
SOUTH	010	SOUTH ST. 010	HAZELWOOD ST	PAVT CHANGE	2	Residential/Local	447	38	16,986	AC			6/14/2021	93
SOUTH	015	SOUTH ST. 015	END OF GD. PAVEMENT 0+00 @001	S.EAST P.L. ROSSI RES. 4+30	2	Residential/Local	430	22	9,460	GRAVEL			-	-
SWHIPP	001	SOUTH WHIPPLE ST. 001	S.F. CURB OAK ST. 0+00	N.SIDE HAZEL RT. OF WAY 12+55	2	Residential/Local	1,255	41	51,455	AC			6/9/2021	89
SWHIPP	002	SOUTH WHIPPLE ST. 002	N.SIDE HAZEL RT OF WAY 0+00	N.F. CURB WALNUT ST.12+85	2	Residential/Local	1,285	32	41,120	AC	1/1/2009	SLURRY SEAL	6/9/2021	83
SPRING	001	SPRING ST.	S.F. CURB CHESTNUT ST 0+00	END OF PAVEMENT 5+04	2	Residential/Local	504	30	15,120	AC/AC	7/1/2019	Mill and Overlay	6/14/2021	100
STEWCI	001	STEWART CIRCLE	W.F. CURB STEWART 0+00	END OF CIRCLE 0+95	2	Residential/Local	95	90	9,000	AC			6/11/2021	39
STEWAR	001	STEWART ST. 001	E.F. CURB OLD HAUL RD. 0+00	N.F. CURB ELM ST 13+30	2	Residential/Local	1,330	40	53,200	AC			6/11/2021	34
STEWAR	002	STEWART ST. 002	S.F. CURB W. ELM 0+00	SPRUCE	2	Residential/Local	413	43	17,759	AC/AC	7/1/2003	AC OVERLAY 2 INCHES	6/11/2021	54
STEWAR	003	STEWART ST. 003	SPRUCE	N.F. CURB PINE ST 16+90	2	Residential/Local	1,380	43	59,340	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/11/2021	86
SUSIE	001	SUSIE COURT	N.F. CURB CHESTNUT ST 0+00	END OF CULDESAC 5+48	2	Residential/Local	548	36	19,728	AC			6/14/2021	26
TAUBOL	001	TAUBOLD COURT	E.F. CURB DANA 0+00	END OF CULDESAC 2+31	2	Residential/Local	231	32	7,392	AC	1/1/2009	SLURRY SEAL	6/12/2021	25
TEMLN	001	TEMPLE LN	FORT-BRAGG SHERWOOD RD	NORTH END	2	Residential/Local	350	18	6,300	GRAVEL			-	-
WALL	001A	WALL ST. 001	S.F.CURB OAK ST 0+00	WILLOW	2	Residential/Local	660	38	25,080	AC/AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/9/2021	95
WALL	001B	WALL ST. 001	WILLOW	N.F.CURB@ALLEY ST 13+50	2	Residential/Local	650	38	24,700	AC/AC	1/1/2009	AC OVERLAY 1.5 INCHES	6/9/2021	80
WALL	002	WALL ST. 002	E.F. CURB @ ALLEY 0+00	N.F. CURB CHESTNUT ST.	2	Residential/Local	700	38	26,600	AC/AC	1/1/2009	AC OVERLAY 1.5 INCHES	6/9/2021	82
WALNUT	003	WALNUT ST.	W.F.CURB S.FRANKLIN 0+00	E.F.CURB HWY1 3+54	2	Residential/Local	354	43	15,222	AC/AC	7/1/2003	AC OVERLAY 2 INCHES	6/14/2021	40
WALNUT	004	WALNUT ST.	GROVE ST	EAST END	2	Residential/Local	188	43	8,084	AC/AC			6/14/2021	52
WALNUT	001	WALNUT ST. 001	W.F.CURB GROVE 0+00	WHIPPLE	2	Residential/Local	340	39	13,260	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	63
WALNUT	002	WALNUT ST. 002	WHIPPLE	E.F.CURB S.FRANKLIN ST. 8+40	2	Residential/Local	1,041	30	31,230	AC/AC	7/1/2003	AC OVERLAY 2 INCHES	6/14/2021	7
WALDER	001	WEST ALDER ST.	W.P.L. N. COAST PLMB. 0+00	W.F. HWY1 1+50	2	Residential/Local	150	43	6,450	AC			6/15/2021	100
WBUSH	001	WEST BUSH ST.	E.F. SIDEWALK @ ALLEY 0+00	W.F. HWY1 8+15	2	Residential/Local	815	43	35,045	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/11/2021	92
WCYPST	001	WEST CYPRESS	WEST END	S MAIN ST	2	Collector	165	50	8,250	AC			6/10/2021	53
WELM	001	WEST ELM ST.	W.F. CURB HWY1 0+00	W.EDGE PAVEMENT 10+30	2	Collector	1,030	43	44,290	AC	8/1/2011	TRIPLE LAYER RUBBERIZED ASPHALT	6/11/2021	75
WFIR	001	WEST FIR ST.	E.F.@ ALLEY 0+00	W.F. HWY1 8+25	2	Collector	900	43	38,700	AC			6/11/2021	7
WLAURE	001	WEST LAUREL ST.	END OF CURB N. SIDE 0+00	W.F. CURB HWY1 1+55	2	Residential/Local	155	43	6,665	AC	1/1/2009	RECONSTRUCT SURFACE (AC)	6/15/2021	97
WOAKST	001	WEST OAK ST.	CHIEF CELERI DR	S MAIN ST	2	Arterial	200	45	9,000	AC			6/14/2021	34
WPINE	001	WEST PINE ST.	E.F. ALLEY 0+00	END SIDEWALK N.SIDE 6+17	2	Residential/Local	617	43	26,531	AC			6/11/2021	93
WPINE	002	WEST PINE ST.	END SIDE WALK N.SIDE 0+00	W.F. CURB HWY 1 2+58	2	Collector	258	43	11,094	AC			6/11/2021	81

City of Fort Brag  
 PCI List by Street Name  
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Street ID	Section ID	Street Name	Beg Location	End Location	Lanes	Functional Class	Length	Width	Area	Surface Type	Last M&R Date	Last M&R Treatment	Inspection Date	PCI
WREDWO	001	WEST REDWOOD AVE.	WEST EDGE ALLEY 0+00	W.F. CURB HWY1 1+85	2	Residential/Local	185	44	8,140	AC			6/15/2021	98
WSPRUC	001	WEST SPRUCE ST.	W.F. CURB WEST ST. 0+00	W.F. HWY1 7+10	2	Residential/Local	710	43	30,530	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/11/2021	95
WEST	001	WEST ST.	N.F. CURB PINE ST. 0+00	S.F. CURB SPRUCE ST. 16+24	2	Residential/Local	1,380	43	59,340	AC			6/11/2021	3
WILLOW	001	WILLOW ST. 001	W.F.CURB S.SANDERSON 0+00	E.EDGE ALLEY LIVINGSTON 6+23	2	Residential/Local	623	32	19,936	AC			6/10/2021	68
WILLOW	002	WILLOW ST. 002	E.EDGE ALLEY LIVINGSTON 0+00	E.F.CURB S LINCOLN 6+00	2	Residential/Local	600	42	25,200	AC			6/10/2021	18
WILLOW	003	WILLOW ST. 003	E.F.CURB S.LINCOLN 0+00	E.F.CURB S HAROLD 6+36	2	Residential/Local	636	32	20,352	AC/AC	7/1/2003	AC OVERLAY 2 INCHES	6/10/2021	21
WINNIF	001	WINNIFRED ST.	W. EDGE OF PAVEMENT 0+00	E.F. CURB BRANDON WAY 8+42	2	Residential/Local	842	34	28,628	AC			6/11/2021	0
WOODLA	001	WOODLAND DRIVE	S.F. CURB CHESTNUT ST. 0+00	END CULDESAC 5+55	2	Residential/Local	555	38	21,090	AC			6/9/2021	93
WOODWA	001	WOODWARD ST.	S EDGE SOUTH ST. 0+00	END ST. @ NAIL 6+50	2	Residential/Local	650	26	16,900	AC/AC	7/1/2019	Mill and Overlay	6/11/2021	100

# ALLEYWAYS

City of Fort Bragg  
PCI List by Alleyways Name  
2021 PMP Update

Street ID	Section ID	Alleyways Name	Beg Location	End Location	Lanes	Functional Class	Length	Width	Area	Surface Type	PCI/M&R Date	PCI
N100D	N100D1	N100D	E OAK ST	E ALDER ST	2	Other (Alleyways)	417	20	8340	AC	7/31/2021	23
N100E	N100E1	N100E	E OAK ST	E ALDER ST	2	Other (Alleyways)	413	16	6608	AC	7/31/2021	8
N100F	N100F1	N100F	E OAK ST	E ALDER ST	2	Other (Alleyways)	413	14	5782	GRAVEL	-	-
N100H	N100H1	N100H	E OAK ST	E ALDER ST	2	Other (Alleyways)	414	16	6624	AC	7/31/2021	4
N100J	N100J1	N100J	E OAK ST	E ALDER ST	2	Other (Alleyways)	418	16	6688	AC	7/31/2021	0
N100K	N100K1	N100K	E OAK ST	E ALDER ST	2	Other (Alleyways)	420	16	6720	GRAVEL	-	-
N100L	N100L1	N100L	E OAK ST	E ALDER ST	2	Other (Alleyways)	428	14	5992	GRAVEL	-	-
N101C	N101C1	N101C	W OAK ST	W ALDER ST	2	Other (Alleyways)	420	16	6720	AC	7/31/2021	19
N101G	N101G1	N101G	E OAK ST	300' N/O E OAK ST	2	Other (Alleyways)	300	16	4800	GRAVEL	-	-
N102C	N102C1	N102C	N101C	MAIN ST	2	Other (Alleyways)	146	26	3796	AC	7/31/2021	18
N102G	N102G1	N102G	300 N/O E OAK ST	E ALDER ST	2	Other (Alleyways)	117	16	1872	AC	12/10/2021	100
N200C	N200C1	N200C	W ALDER ST	W REDWOOD AVE	2	Other (Alleyways)	439	17	7463	AC	8/2/2021	11
N200D	N200D1	N200D	E ALDER ST	E REDWOOD AVE	2	Other (Alleyways)	405	18	7290	AC	7/31/2021	22
N200E	N200E1	N200E	E ALDER ST	E REDWOOD AVE	2	Other (Alleyways)	418	12	5016	PCC	8/2/2021	100
N200F	N200F1	N200F	E ALDER ST	E REDWOOD AVE	2	Other (Alleyways)	407	14	5698	AC	7/31/2021	6
N200G	N200G1	N200G	E ALDER ST	E REDWOOD AVE	2	Other (Alleyways)	423	14	5922	GRAVEL	-	-
N200H	N200H1	N200H	E ALDER ST	E REDWOOD AVE	2	Other (Alleyways)	424	14	5936	AC	7/31/2021	4
N200J	N200J1	N200J	E ALDER ST	E REDWOOD AVE	2	Other (Alleyways)	425	16	6800	AC	8/4/2021	0
N200K	N200K1	N200K	CEDAR ST	E REDWOOD AVE	2	Other (Alleyways)	194	14	2716	GRAVEL	-	-
N200L	N200L1	N200L	MORROW ST	E END	2	Other (Alleyways)	200	12	2400	GRAVEL	-	-
N300D	N300D1	N300D	E REDWOOD AVE	E LAUREL ST	2	Other (Alleyways)	413	18	7434	AC	7/31/2021	8
N300E	N300E1	N300E	E REDWOOD AVE	E LAUREL ST	2	Other (Alleyways)	416	18	7488	AC	7/31/2021	100
N300F	N300F1	N300F	E REDWOOD AVE	E LAUREL ST	2	Other (Alleyways)	414	14	5796	AC	7/31/2021	0
N300G	N300G1	N300G	E REDWOOD AVE	N END	2	Other (Alleyways)	168	14	2352	GRAVEL	-	-
N300H	N300H1	N300H	E REDWOOD AVE	E LAUREL ST	2	Other (Alleyways)	426	16	6816	AC	7/31/2021	4
N300J	N300J1	N300J	E REDWOOD AVE	E LAUREL ST	2	Other (Alleyways)	419	16	6704	AC	7/31/2021	8
N300K	N300K1	N300K	E REDWOOD AVE	E LAUREL ST	2	Other (Alleyways)	431	16	6896	GRAVEL	-	-
N400D	N400D1	N400D	E LAUREL ST	E PINE ST	2	Other (Alleyways)	410	20	8200	AC	7/31/2021	0
N400E	N400E1	N400E	E LAUREL ST	E PINE ST	1	Other (Alleyways)	426	18	7668	AC	7/31/2021	1
N400F	N400F1	N400F	E LAUREL ST	E PINE ST	2	Other (Alleyways)	426	16	6816	GRAVEL	-	-
N400H	N400H1	N400H	E LAUREL ST	E PINE ST	2	Other (Alleyways)	414	16	6624	AC	7/31/2021	4
N400J	N400J1	N400J	E LAUREL ST	E PINE ST	2	Other (Alleyways)	424	16	6784	AC	7/31/2021	3
N401G	N401G1	N401G	214' N/O LAUREL ST	E PINE ST	2	Other (Alleyways)	206	14	2884	AC	7/31/2021	22
N402G	N402G1	N402G	E LAUREL ST	214' N/O LAUREL ST	2	Other (Alleyways)	214	14	2996	AC	8/4/2021	1
N500A	N500A1	N500A	W PINE ST	W FIR ST	2	Other (Alleyways)	426	18	7668	GRAVEL	-	-
N500B	N500B1	N500B	W PINE ST	W FIR ST	2	Other (Alleyways)	427	16	6832	GRAVEL	-	-
N500D	N500D1	N500D	E PINE ST	E FIR ST	2	Other (Alleyways)	407	18	7326	AC	8/2/2021	8
N500E	N500E1	N500E	E PINE ST	E FIR ST	2	Other (Alleyways)	418	16	6688	AC	8/2/2021	0
N500F	N500F1	N500F	E PINE ST	E FIR ST	2	Other (Alleyways)	417	16	6672	AC	8/2/2021	0
N500G	N500G1	N500G	E PINE ST	E FIR ST	2	Other (Alleyways)	428	16	6848	AC	8/2/2021	4
N500H	N500H1	N500H	E PINE ST	E FIR ST	2	Other (Alleyways)	422	12	5064	GRAVEL	-	-
N500J	N500J1	N500J	E PINE ST	E FIR ST	2	Other (Alleyways)	427	18	7686	AC	7/31/2021	3
N600A	N600A1	N600A	W FIR ST	W BUSH ST	2	Other (Alleyways)	460	16	7360	GRAVEL	-	-
N600B	N600B1	N600B	W FIR ST	W BUSH ST	2	Other (Alleyways)	416	16	6656	AC	8/2/2021	5
N600C	N600C1	N600C	W FIR ST	W BUSH ST	2	Other (Alleyways)	424	16	6784	AC	8/2/2021	30
N600F	N600F1	N600F	E FIR ST	E BUSH ST	2	Other (Alleyways)	417	16	6672	GRAVEL	-	-
N600G	N600G1	N600G	E FIR ST	E BUSH ST	2	Other (Alleyways)	422	17	7174	AC	8/2/2021	11

City of Fort Bragg  
PCI List by Alleyways Name  
2021 PMP Update

Street ID	Section ID	Alleyways Name	Beg Location	End Location	Lanes	Functional Class	Length	Width	Area	Surface Type	PCI/M&R Date	PCI
N600H	N600H1	N600H	E FIR ST	E BUSH ST	2	Other (Alleyways)	426	14	5964	GRAVEL	-	-
N600J	N600J1	N600J	E FIR ST	N END	2	Other (Alleyways)	662	14	9268	AC	8/4/2021	39
N700A	N700A1	N700A	W BUSH ST	W SPRUCE ST	2	Other (Alleyways)	458	16	7328	GRAVEL	-	-
N700B	N700B1	N700B	W BUSH ST	W SPRUCE ST	2	Other (Alleyways)	425	14	5950	GRAVEL	-	-
N700C	N700C1	N700C	W BUSH ST	W SPRUCE ST	2	Other (Alleyways)	409	14	5726	GRAVEL	-	-
N700G	N700G1	N700G	E BUSH ST	WINIFRED ST	2	Other (Alleyways)	268	14	3752	GRAVEL	-	-
N700H	N700H1	N700H	E BUSH ST	WINIFRED ST	2	Other (Alleyways)	264	18	4752	GRAVEL	-	-
N800C	N800C1	N800C	W SPRUCE ST	W ELM ST	2	Other (Alleyways)	422	16	6752	AC	8/4/2021	100
N800D	N800D1	N800D	E SPRUCE ST	E ELM ST	2	Other (Alleyways)	423	18	7614	AC	8/2/2021	21
N800G	N800G1	N800G	WINIFRED ST	N END	2	Other (Alleyways)	358	14	5012	GRAVEL	-	-
N900D	N900D1	N900D	E ELM ST	E MANZANITA ST	2	Other (Alleyways)	405	20	8100	AC	8/2/2021	8
S100D	S100D1	S100D	S END	E OAK ST	2	Other (Alleyways)	20	16	320	AC	-	-
S100E	S100E1	S100E	MADRONE ST	E OAK ST	2	Other (Alleyways)	417	16	6672	AC	8/3/2021	1
S100F	S100F1	S100F	MADRONE ST	E OAK ST	2	Other (Alleyways)	418	18	7524	GRAVEL	-	-
S100G	S100G1	S100G	MADRONE ST	E OAK ST	2	Other (Alleyways)	418	10	4180	PCC	8/2/2021	100
S100H	S100H1	S100H	MADRONE ST	E OAK ST	2	Other (Alleyways)	410	14	5740	AC	12/10/2021	100
S100J	S100J1	S100J	MADRONE ST	E OAK ST	2	Other (Alleyways)	418	14	5852	AC	8/2/2021	98
S100K	S100K1	S100K	WILLOW ST	E OAK ST	2	Other (Alleyways)	606	14	8484	AC	8/3/2021	4
S100L	S100L1	S100L	WILLOW ST	E OAK ST	2	Other (Alleyways)	608	14	8512	AC	8/3/2021	0
S100M	S100M1	S100M	WILLOW ST	E OAK ST	2	Other (Alleyways)	603	14	8442	AC	8/3/2021	6
S100N	S100N1	S100N	WILLOW ST	E OAK ST	2	Other (Alleyways)	616	16	9856	GRAVEL	-	-
S100P	S100P1	S100P	WILLOW ST	E OAK ST	2	Other (Alleyways)	605	16	9680	AC	8/3/2021	1
S200D	S200D1	S200D	MAPLE ST	MADRONE ST	2	Other (Alleyways)	425	16	6800	AC	8/3/2021	16
S200E	S200E1	S200E	MAPLE ST	MADRONE ST	2	Other (Alleyways)	402	17	6834	AC	8/3/2021	1
S200G	S200G1	S200G	MAPLE ST	MADRONE ST	2	Other (Alleyways)	425	14	5950	AC	8/3/2021	16
S200H	S200H1	S200H	MAPLE ST	MADRONE ST	2	Other (Alleyways)	410	14	5740	GRAVEL	-	-
S200K	S200K1	S200K	MAPLE ST	WILLOW ST	2	Other (Alleyways)	258	16	4128	GRAVEL	-	-
S200L	S200L1	S200L	MAPLE ST	WILLOW ST	2	Other (Alleyways)	242	14	3388	AC	8/3/2021	0
S201F	S201F1	S201F	105' N/O MAPLE ST	MADRONE ST	2	Other (Alleyways)	309	12	3708	AC	8/3/2021	34
S202F	S202F1	S202F	MAPLE ST	105' N/O MAPLE ST	2	Other (Alleyways)	105	14	1470	AC	8/3/2021	3
S300D	S300D1	S300D	HAZEL ST	MAPLE ST	2	Other (Alleyways)	358	16	5728	AC	8/3/2021	10
S300E	S300E1	S300E	HAZEL ST	MAPLE ST	2	Other (Alleyways)	356	16	5696	AC	8/3/2021	20
S300F	S300F1	S300F	HAZEL ST	MAPLE ST	2	Other (Alleyways)	363	10	3630	GRAVEL	-	-
S300G	S300G1	S300G	HAZEL ST	MAPLE ST	2	Other (Alleyways)	373	16	5968	AC	12/10/2021	100
S300H	S300H1	S300H	S END	MAPLE ST	2	Other (Alleyways)	364	15	5460	GRAVEL	-	-
S300J	S300J1	S300J	S END	MAPLE ST	2	Other (Alleyways)	356	14	4984	AC	8/3/2021	4
S300K	S300K1	S300K	S END	MAPLE ST	2	Other (Alleyways)	579	13	7527	AC	12/10/2021	100
S300P	S300P1	S300P	WALL ST	WILLOW ST	2	Other (Alleyways)	955	20	19100	AC	8/3/2021	6
S301N	S301N1	S301N	S302N	WILLOW ST	2	Other (Alleyways)	652	12	7824	AC	8/3/2021	96
S302N	S302N1	S302N	S301N	WALL ST	2	Other (Alleyways)	139	16	2224	AC	8/4/2021	98
S400H	S400H1	S400H	E CHESTNUT ST	N END	2	Other (Alleyways)	641	14	8974	AC	8/3/2021	0
S400K	S400K1	S400K	E CHESTNUT ST	N END	2	Other (Alleyways)	276	20	5520	AC	8/4/2021	24
S500D	S500D1	S500D	WALNUT ST	E CHESTNUT ST	2	Other (Alleyways)	614	16	9824	AC	8/2/2021	98
S500H	S500H1	S500H	WALNUT ST	E CHESTNUT ST	2	Other (Alleyways)	635	18	11430	AC	8/2/2021	98
S500J	S500J1	S500J	WALNUT ST	E CHESTNUT ST	2	Other (Alleyways)	378	18	6804	GRAVEL	-	-
S600E	S600E1	S600E	E END	WALNUT ST	2	Other (Alleyways)	610	12	7320	AC	12/10/2021	100
S800E	S800E1	S800E	N HARBOR DR	SOUTH ST	2	Other (Alleyways)	592	16	9472	AC	8/2/2021	4

## **Section Description Inventory – Sorted by Descending PCI**

**STREETS**

City of Fort Brag  
 PCI List by Descending PCI  
 2021 PMP Update

Street ID	Section ID	Street Name	Beg Location	End Location	Lanes	Functional Class	Length	Width	Area	Surface Type	Last M&R Date	Last M&R Treatment	Inspection Date	PCI
ALGER	001	ALGER ST.	N.F.CURB LAUREL 0+00	END OF ST. 5+64	2	Residential/Local	564	22	12,408	AC/AC	7/1/2019	Mill and Overlay	6/12/2021	100
DUBOIS	001	DUBOIS LANE	S.F. CURB WALNUT 0+00	END OF PAVEMENT 2+07	2	Residential/Local	207	25	5,175	AC/AC	7/1/2019	Mill and Overlay	6/9/2021	100
EFIR	003	EAST FIR ST. 002	RR TRACKS	W.F.CURB H FRANKLIN	2	Collector	192	44	8,448	AC/AC	7/1/2019	Mill and Overlay	6/11/2021	100
ESPEY	001	ESPEY WAY	S.F. CURB WALNUT ST 0+00	END OF PAVEMENT 2+58	2	Residential/Local	258	36	9,288	AC/AC	7/1/2019	Mill and Overlay	6/9/2021	100
HALSEY	001	HALSEY WAY	E.F. CURB MCKINLEY ST.0+00	E.F. CURB HALSY WAY 2+85	2	Residential/Local	285	32	9,120	AC/AC	7/1/2019	Mill and Overlay	6/12/2021	100
HAZEL	003	HAZEL ST.	E.F.CURB S. MCPHERSON 0+00	W.F.CURB S.HARRISON 2+95	2	Residential/Local	295	27	7,965	AC			6/10/2021	100
MYRTLE	001	MYRTLE ST.	S.F. N. HARBOR DR. 0+00	S.EDGE SOUTH ST. 5+55	2	Residential/Local	555	26	14,430	AC			6/11/2021	100
NESS	001	NESS AVE.	E.F. CURB SANDERSON WAY 0+00	E. P.L. 111 NESS FENCE 2+45	2	Residential/Local	245	22	5,390	AC			6/12/2021	100
NHAROL	001	NORTH HAROLD ST. 001	END OF PAVEMENT 0+00	N.S.DRVWY BEHIND M.SCHOOL3+16	2	Residential/Local	316	28	8,848	AC/AC	7/1/2019	Mill and Overlay	6/11/2021	100
NHAROL	002	NORTH HAROLD ST. 002	N.SIDE M.S.SCH.DRVWAY 0+00	N.CURB @ FIR ST.2+85	2	Residential/Local	285	40	11,400	AC/AC	7/1/2019	Mill and Overlay	6/11/2021	100
NSANDE	002	NORTH SANDERSON WAY 002	N.F. CURB CEDAR ST.0+00	S. P.L. 222 N SANDERSON 7+27	2	Residential/Local	727	20	14,540	AC/AC	7/1/2017	Mill and Overlay	6/12/2021	100
OLSEN	001	OLSEN LANE	S.F. CURB CHESTNUT 0+00	END OF ST. 6+52	2	Residential/Local	652	28	18,256	AC/AC	7/1/2019	Mill and Overlay	6/9/2021	100
RASSMU	001	RASSMUSSEN LANE	N EDGE CEDAR 0+00	END OF GOOD PAVEMENT 4+55	2	Residential/Local	455	13	5,915	AC/AC	7/1/2019	Mill and Overlay	6/12/2021	100
SPRING	001	SPRING ST.	S.F. CURB CHESTNUT ST 0+00	END OF PAVEMENT 5+04	2	Residential/Local	504	30	15,120	AC/AC	7/1/2019	Mill and Overlay	6/14/2021	100
WALDER	001	WEST ALDER ST.	W.P.L. N. COAST PLMB. 0+00	W.F. HWY1 1+50	2	Residential/Local	150	43	6,450	AC			6/15/2021	100
WOODWA	001	WOODWARD ST.	S EDGE SOUTH ST. 0+00	END ST. @ NAIL 6+50	2	Residential/Local	650	26	16,900	AC/AC	7/1/2019	Mill and Overlay	6/11/2021	100
BOATYA	001	BOATYARD DR.	5+30 FORM HWY1 0+00	4+32 FROM BEGIN	2	Arterial	432	24	10,368	AC/AC	7/1/2017	Mill and Overlay	6/10/2021	99
NOYOPT	001	NOYO POINT RD	CYPRESS ST	END AT LOT	2	Residential/Local	1,828	24	43,872	AC			6/15/2021	98
WREDWO	001	WEST REDWOOD AVE.	WEST EDGE ALLEY 0+00	W.F. CURB HWY1 1+85	2	Residential/Local	185	44	8,140	AC			6/15/2021	98
AIRPOR	001A	AIRPORT ROAD	C.L. SIGN 0+00	W/C.L. SIGN 5+87	2	Residential/Local	587	22	12,914	AC			6/12/2021	97
AIRPOR	001B	AIRPORT ROAD	W/C.L. SIGN 5+87	E.EDGE HWY1 @ LIMIT LINE 8+55	2	Residential/Local	268	22	5,896	AC			6/12/2021	97
WLAURE	001	WEST LAUREL ST.	END OF CURB N. SIDE 0+00	W.F. CURB HWY1 1+55	2	Residential/Local	155	43	6,665	AC	1/1/2009	RECONSTRUCT SURFACE (AC)	6/15/2021	97
KEMPPE	005	KEMPEE WAY	E.F. HOSPITAL WY 0+00	433 FT E/O HOSPITAL WY	2	Residential/Local	433	37	16,021	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	96
SNUGH	001	SNUG HARBOR PLACE	OCEAN VIEW DR	SOUTH END	2	Residential/Local	110	19	2,090	AC			6/15/2021	96
EBUSH	001	EAST BUSH ST.	W.F. CURB BRANDON WAY 0+00	ES.ALLEY PRKINS&HARISON 12+23	2	Residential/Local	471	33	15,543	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/10/2021	95
EBBING	001	EBBING WAY	S.F. CURB CHESTNUT 0+00	END OF CULDESAC 5+62	2	Residential/Local	562	37	20,794	AC			6/9/2021	95
FERN	001	FERN AVE.	E.F. CURB MORROW ST 0+00	E.F. CURB END OF ST. 3+22	2	Residential/Local	322	29	9,338	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/12/2021	95
GROVE	001	GROVE ST.	N.F. CURB WALNUT 0+00	S.F. CURB CHESTNUT ST 6+25	2	Residential/Local	625	30	18,750	AC	1/1/2009	SLURRY SEAL	6/14/2021	95
HAZEL	002	HAZEL ST.	E.F.CURB S. FRANKLIN 0+00	W.F.CURB S.McPHERSON 3+15	2	Residential/Local	315	32	10,080	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/10/2021	95
MORROW	001	MORROW ST. 001	N.F. CURB OAK ST.0+00	S.F. CURB CEDAR 6+54	2	Residential/Local	654	34	22,236	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/15/2021	95
RIVER	001	RIVER DRIVE	N.F. RIVER DR. 0+00	N. CURB HOSPITAL DR. 5+15	2	Residential/Local	515	44	22,660	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	95
SCORRY	001	SOUTH CORRY ST. 001	N.F. CURB CHESTNUT ST. 0+00	S.F. CURB OAK ST.19+37	2	Residential/Local	1,937	38	73,606	AC			6/9/2021	95
SMcPHE	001	SOUTH McPHERSON ST.	S.F. CURB OAK ST.	MADRONE	2	Collector	436	44	19,184	AC			6/15/2021	95
WALL	001A	WALL ST. 001	S.F.CURB OAK ST 0+00	WILLOW	2	Residential/Local	660	38	25,080	AC/AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/9/2021	95
WSPRUC	001	WEST SPRUCE ST.	W.F. CURB WEST ST. 0+00	W.F. HWY1 7+10	2	Residential/Local	710	43	30,530	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/11/2021	95
MORROW	002	MORROW ST. 002	N.F. CURB CEDAR ST.0+00	S.F. CURB LAUREL 6+56	2	Residential/Local	656	42	27,552	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/15/2021	94
SHAROL	002	SOUTH HAROLD ST. 002	S.F. MAPLE ST. 0+00	N.F. CURB CHESTNUT ST. 9+75	2	Residential/Local	975	20	19,500	AC/AC	1/1/2009	AC OVERLAY 1.5 INCHES	6/14/2021	94
EBUSH	010	EAST BUSH ST.	N. FRANKLIN ST	E.F. CURB HWY1 12+23	2	Residential/Local	345	43	14,835	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/10/2021	93
MADRON	005	MADRONE ST. 002	E.F. CURB @S MCPHERSON ST 0+00	CORRY ST	2	Residential/Local	900	44	39,600	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/10/2021	93
NSANDE	001	NORTH SANDERSON WAY 001	N.F. CURB OAK ST.0+00	S.F. CURB CEDAR 6+31	2	Residential/Local	631	29	18,299	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/12/2021	93
EREDWO	010	REDWOOD AVE (EAST) 001	N. HAROLD ST	E.F. MORROW ST	2	Residential/Local	401	44	17,644	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/12/2021	93
SLINCO	015B	SOUTH LINCOLN ST.	WILLOW ST	S.F. CURB OAK ST	2	Residential/Local	494	30	14,820	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	93
SSANDE	005	SOUTH SANDERSON ST. 002	END OF W. SIDEWALK 0+00	412 FT N/O END OF W. SIDEWALK	2	Residential/Local	412	30	12,360	AC			6/9/2021	93



City of Fort Brag  
PCI List by Descending PCI  
2021 PMP Update

Street ID	Section ID	Street Name	Beg Location	End Location	Lanes	Functional Class	Length	Width	Area	Surface Type	Last M&R Date	Last M&R Treatment	Inspection Date	PCI
SOUTH	010	SOUTH ST. 010	HAZELWOOD ST	PAVT CHANGE	2	Residential/Local	447	38	16,986	AC			6/14/2021	93
WPINE	001	WEST PINE ST.	E.F. ALLEY 0+00	END SIDEWALK N.SIDE 6+17	2	Residential/Local	617	43	26,531	AC			6/11/2021	93
WOODLA	001	WOODLAND DRIVE	S.F. CURB CHESTNUT ST. 0+00	END CULDESAC 5+55	2	Residential/Local	555	38	21,090	AC			6/9/2021	93
MINNES	001	MINNESOTA AVE.	S.F. CURB CHESTNUT ST. 0+00	CITY LIMITS 6+30	2	Residential/Local	630	32	20,160	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/9/2021	92
SDANA	001	SOUTH DANA ST.	N.F. CUEB CHESTNUT ST 0+00	S.F. CURB OAK ST. 19+00	2	Collector	1,900	40	76,000	AC	8/1/2011	TRIPLE LAYER RUBBERIZED ASPHALT	6/10/2021	92
SSANDE	001	SOUTH SANDERSON ST. 001	N.F. CURB CHESTNUT ST 0+00	S.F. CURB OAK ST 19+15	2	Collector	1,915	40	76,600	AC			6/9/2021	92
WBUSH	001	WEST BUSH ST.	E.F. SIDEWALK @ ALLEY 0+00	W.F. HWY1 8+15	2	Residential/Local	815	43	35,045	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/11/2021	92
ELAURE	005	EAST LAUREL ST.003	W.F. CURB N. HAROLD 0+00	MORROW ST	2	Residential/Local	412	40	16,480	AC			6/12/2021	91
SMCPHE	003	SOUTH McPHERSON ST.	MAPLE	N.F. CURB CHESTNUT ST.	2	Collector	1,042	44	45,848	AC			6/15/2021	91
ELAURE	010	EAST LAUREL ST.003	MORROW ST	W.F. CURB END ST. @PARK 8+30	2	Residential/Local	413	40	16,520	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/12/2021	90
PUDDIN	001	PUDDING CREEK RD.	C.L SIGN0+00	E.F. HWY1 @ LIMIT LINE 9+55	2	Residential/Local	955	23	21,965	AC	6/1/2010	RECONSTRUCT SURFACE (AC)	6/11/2021	90
DELM	002	DELMAR DR	OCEAN VIEW DR	END SOUTH	2	Residential/Local	242	30	7,260	AC			6/10/2021	89
EBUSH	005	EAST BUSH ST.	ES.ALLEY PRKINSHARISON 0+00	N. FRANKLIN ST	2	Residential/Local	890	43	38,270	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/10/2021	89
SWHIPP	001	SOUTH WHIPPLE ST. 001	S.F. CURB OAK ST. 0+00	N.SIDE HAZEL RT. OF WAY 12+55	2	Residential/Local	1,255	41	51,455	AC			6/9/2021	89
ELAURE	001	EAST LAUREL ST. 001	E.F. CURB HWY1 0+00	W.F. CURB HARRISON ST 9+25	2	Collector	925	45	41,625	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/12/2021	88
OAKTER	001	OAK TERRACE COURT	N.F. CURB OAK ST. 0+00	END OF ST. 3+38	2	Residential/Local	338	36	12,168	AC	1/1/2009	SLURRY SEAL	6/12/2021	86
STEWAR	003	STEWART ST. 003	SPRUCE	N.F. CURB PINE ST 16+90	2	Residential/Local	1,380	43	59,340	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/11/2021	86
BRAND0	001	BRANDON WAY	N.F. CURB E. FIR ST. 0+00	GUARD BARRIER @ WINIFRED 7+28	2	Residential/Local	728	34	24,752	AC			6/10/2021	85
NWHIPP	002	NORTH WHIPPLE ST.	ALDER ST	S.F.CURB FIR ST.	2	Residential/Local	1,850	44	81,400	AC			6/11/2021	85
SSANDE	010	SOUTH SANDERSON ST. 002	412 FT N/O END OF W. SIDEWALK	S.F. CURB CHESTNUT ST 1040	2	Residential/Local	628	30	18,840	AC			6/9/2021	85
CHESTN	005	CHESTNUT ST.	END CURB-GUTTER@001,0+00	S. WHIPPLE	2	Collector	884	32	28,288	AC/AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/9/2021	84
DELM	001	DELMAR DR	OCEAN VIEW DR	END NORTH	2	Residential/Local	755	21	15,855	ST			6/10/2021	84
EALDER	001	EAST ALDER 001	E.F CURB HWY 1 0+00	W.F.CURB N.FRANKLIN ST. 3+40	1	Collector	340	43	14,620	AC	1/1/2009	RECONSTRUCT SURFACE (AC)	6/10/2021	84
EDDY	001	EDDY ST.	W.F. CURB S SANDERSON 0+00	END OF STREET 3+18	2	Residential/Local	318	41	13,038	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/9/2021	83
FLOREN	001	FLORENCE ST.	S.F. CURB OAK 0+00	N.F. CURB WILLOW ST. 6+03	2	Residential/Local	603	31	18,693	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	83
OCEANV	003	OCEAN VIEW DRV.EAST 003	E.F. CURB HWY 1 0+00	5+30 FROM HWY1	2	Residential/Local	530	26	13,780	AC			6/10/2021	83
SWHIPP	002	SOUTH WHIPPLE ST. 002	N.SIDE HAZEL RT OF WAY 0+00	N.F. CURB WALNUT ST.12+85	2	Residential/Local	1,285	32	41,120	AC	1/1/2009	SLURRY SEAL	6/9/2021	83
EALDER	002B	EAST ALDER 002	HAROLD	@HYDRANT END OF ST. 28+45	1	Residential/Local	1,261	43	54,223	AC	1/1/2009	RUBB CHIP SEAL/SLURRY	6/10/2021	82
ECYPRES	001	EAST CYPRESS ST. 001	END OF KEMPEE @ VALVE BOX 0+0	W.F.CURB RIVER DRV 12+96	2	Collector	1,296	40	51,840	AC/AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	82
OAK	001	EAST OAK ST. 001	E.F. CURB HWY 1 0+00	W.F.CURB MCKINLEY 32+25	2	Arterial	3,225	45	145,125	AC	1/1/2009	SLURRY SEAL	6/9/2021	82
MAPLE	005B	MAPLE ST. 005	S. FRANKLIN	E.F. CURB HWY 1	2	Collector	400	49	19,600	AC			6/10/2021	82
NHARBO	002	NORTH HARBOR DRIVE 002	E.F.CURB S FRANKLIN ST. 0+00	E.F.CURB HWY 1 4+55	2	Residential/Local	455	32	14,560	AC			6/11/2021	82
NHARRI	001A	NORTH HARRISON ST.	N.EDGE PAVEMENT @END ST0+00	REDWOOD AVE (EAST)	2	Collector	2,256	44	99,264	AC			6/15/2021	82
PARK	002	PARK ST. 002	S.F. CURB WILLOW 0+00	WHERE ST.NARROWS 300 BLK 5+68	2	Residential/Local	568	28	15,904	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	82
WALL	002	WALL ST. 002	E.F. CURB @ ALLEY 0+00	N.F. CURB CHESTNUT ST.	2	Residential/Local	700	38	26,600	AC/AC	1/1/2009	AC OVERLAY 1.5 INCHES	6/9/2021	82
NHAROL	005	NORTH HAROLD ST. 005	S.F. CURB FIR ST 0+00	LAUREL ST	2	Collector	920	52	47,840	AC	8/1/2011	TRIPLE LAYER RUBBERIZED ASPHALT	6/11/2021	81
SOUTH	005	SOUTH ST. 005	E. EDGE VALLEY GUTTER HWY 1	HAZELWOOD ST	2	Collector	1,218	38	46,284	AC			6/14/2021	81
WPINE	002	WEST PINE ST.	END SIDE WALK N.SIDE 0+00	W.F. CURB HWY 1 2+58	2	Collector	258	43	11,094	AC			6/11/2021	81
EPINE	010	EAST PINE ST.	CORRY ST	W.F. CURB HAROLD ST 19+30	2	Collector	356	43	15,308	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/15/2021	80
NCORRY	001	NORTH CORRY ST.	N.F.CURB OAK ST.0+00	S.F.CURB FIR ST. 22+47	2	Residential/Local	2,247	42	94,374	AC			6/10/2021	80
NMcPHE	001	NORTH McPHERSON ST.	N.F. CURB OAK ST. 0+00	S.F. CURB BUSH ST. 27+08	2	Collector	2,708	44	119,152	AC			6/15/2021	80

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Street ID	Section ID	Street Name	Beg Location	End Location	Lanes	Functional Class	Length	Width	Area	Surface Type	Last M&R Date	Last M&R Treatment	Inspection Date	PCI
PERKIN	001	PERKINS WAY	N.F. CURB E. FIR ST. 0+00	@ DEAD END10+53	2	Residential/Local	1,053	33	34,749	AC			6/11/2021	80
WALL	001B	WALL ST. 001	WILLOW	N.F.CURB@ALLEY ST 13+50	2	Residential/Local	650	38	24,700	AC/AC	1/1/2009	AC OVERLAY 1.5 INCHES	6/9/2021	80
EFIR	001	EAST FIR ST. 001	E.F.CURB @ N FRANKLIN ST	E.F.CURB HAROLD 17+45	2	Collector	1,745	43	75,035	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/11/2021	78
OAK	002	EAST OAK ST. 002	E.F. CURB MCKINLEY ST. 0+00	BACK E.SIDE SDWLK. DANA 17+25	2	Residential/Local	1,725	40	69,000	AC	1/1/2009	SLURRY SEAL	6/10/2021	78
EELM	001	EAST ELM ST.	E.F.@ HWY1 0+00	W.F. CURB N FRANKLIN ST 3+25	2	Collector	325	44	14,300	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/11/2021	77
ELAURE	002	EAST LAUREL 002	E.F.CURB N.HARRISON 0+00	W.F.CURB N.HAROLD 10+05	2	Collector	1,005	40	40,200	AC/AC	1/1/2009	AC OVERLAY 1.5 INCHES	6/12/2021	76
EALDER	002A	EAST ALDER 002	E.F.CURB S.FRANKLIN 0+00	HAROLD	1	Collector	1,584	43	68,112	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/10/2021	75
NHAROL	006	NORTH HAROLD ST. 006	LAUREL ST	ALDER ST	2	Collector	920	52	47,840	AC/AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/11/2021	75
WELM	001	WEST ELM ST.	W.F. CURB HWY1 0+00	W.EDGE PAVEMENT 10+30	2	Collector	1,030	43	44,290	AC	8/1/2011	TRIPLE LAYER RUBBERIZED ASPHALT	6/11/2021	75
CHESTN	015	CHESTNUT ST.	SANDERSON WY	END OF STREET (9+75)	2	Collector	975	32	31,200	AC/AC	1/1/2009	AC OVERLAY 1.5 INCHES	6/9/2021	74
JEWETT	001	JEWETT ST.	S.F.SIDEWALK OAK ST.0+00	END OF CULDESAC 5+36	2	Residential/Local	536	39	20,904	AC	1/1/2009	SLURRY SEAL	6/12/2021	73
LIVING	001	LIVINGSTON ST.	S.F.CURB OAK 0+00	N.F.CURB WILLOW ST.6+17	2	Residential/Local	617	35	21,595	AC/AC	1/1/2009	AC OVERLAY 1.5 INCHES	6/14/2021	73
NHARBO	001	NORTH HARBOR DRIVE 001	CITY LIMITS @GUARD RAIL 0+00	E.F.CURB S. FRANKLIN 19+90	2	Residential/Local	1,990	28	55,720	AC			6/11/2021	73
NHARRI	001B	NORTH HARRISON ST.	REDWOOD AVE (EAST)	N.F. CURB OAK ST	2	Collector	1,056	44	46,464	AC			6/15/2021	72
MAPLE	001B	MAPLE ST. 001	S.HAROLD	ALLEY BETWN WHIPPLE HARRISON	2	Collector	1,000	45	45,000	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/10/2021	71
EREDWO	005	REDWOOD AVE (EAST) 001	E.F. CURB HWY 1	N. HAROLD ST	2	Arterial	1,929	44	84,876	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/12/2021	71
MANZAN	001	MANZANITA ST.	W.F. CURB N. FRANKLIN ST.0+00	E.F. CURB HWY1 3+30	2	Residential/Local	330	26	8,580	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/11/2021	70
MAPLE	005A	MAPLE ST. 005	ALLEY BETWN WHIPPLE HARRISON	S. FRANKLIN	2	Collector	900	49	44,100	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/10/2021	70
SLINCO	015A	SOUTH LINCOLN ST.	MAPLE ST	WILLOW ST	2	Collector	282	30	8,460	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	70
OCEANV	001	OCEAN VIEW DRV.WEST 001	W.F. CURB HWY1 0+00	4+18 FROM W.F. HWY1	2	Residential/Local	418	36	15,048	AC/AC	8/1/2013	AC OVERLAY 2 INCHES	6/10/2021	69
CHESTN	010	CHESTNUT ST.	S. WHIPPLE	SANDERSON WY	2	Collector	2,937	32	93,984	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/10/2021	68
MADRON	001	MADRONE ST. 001	E.F.CURB @ HWY 1 0+00	W.F.CURB @ McPHERSON ST.6+50	2	Residential/Local	650	43	27,950	AC			6/10/2021	68
WILLOW	001	WILLOW ST. 001	W.F.CURB S.SANDERSON 0+00	E.EDGE ALLEY LIVINGSTON 6+23	2	Residential/Local	623	32	19,936	AC			6/10/2021	68
PARK	003	PARK ST. 003	END OF 002 0+00	END OF ST. 1+45	1	Residential/Local	145	14	2,030	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	67
HAZELW	001	HAZELWOOD ST.	S.F. SOUTH ST. 0+00	HYDRANT @ END OF ST.3+60	2	Residential/Local	360	28	10,080	AC			6/14/2021	65
SHARRI	001	SOUTH HARRISON ST.	N.F. CURB WALNUT ST. 0+00	S.F. CURB OAK ST. 25+45	2	Collector	2,545	44	111,980	AC	1/1/2009	SLURRY SEAL	6/15/2021	65
MADRON	010	MADRONE ST. 002	CORRY ST	W.F. CURB @ HAROLD 13+00	2	Residential/Local	400	44	17,600	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/10/2021	64
NFRANK	001	NORTH FRANKLIN ST. 001	N.F.CURB @ E.ALDER 0+00	S.F.CURB @ E. FIR 17+77	2	Arterial	1,777	43	76,411	AC	6/1/2013	4" DIGOUTS	6/14/2021	63
WALNUT	001	WALNUT ST. 001	W.F.CURB GROVE 0+00	WHIPPLE	2	Residential/Local	340	39	13,260	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	63
OAK	003	EAST OAK ST. 003	BACK E.SIDE SDWLK DANA 0+00	CITY LIMIT SIGN OAK 16+10	2	Residential/Local	1,610	24	38,640	AC	1/1/2009	SLURRY SEAL	6/10/2021	62
NDANA	001	NORTH DANA ST	EAST OAK ST	END	2	Residential/Local	319	28	8,932	AC			6/12/2021	61
SFRANK	003	SOUTH FRANKLIN ST. 003	N.F.CURB @ SOUTH 0+00	N.F.CURB @ S.HARBOR 5+65	2	Arterial	565	43	24,295	AC/AC	7/1/2019	Mill and Overlay	6/14/2021	61
FRONTA	001	FRONTAGE ROAD(UNNAMED)	N.END @ LIMIT LINE 0+00	N.F. CURB OCEAN VIEW. W 6+80	2	Residential/Local	680	24	16,320	AC			6/16/2021	60
KEMPPE	010	KEMPPE WAY	433 FT E/O HOSPITAL WY	N. OF VALVE BOX 9+35	2	Residential/Local	502	37	18,574	AC	6/1/2013	TRIPLE LAYER RUBBERIZED ASPHALT	6/14/2021	60
NFRANK	004	NORTH FRANKLIN ST. 004	N.F.CURB @ OAK ST 0+00	N.F.CURB @ E.ALDER ST. 4+70	2	Arterial	470	43	20,210	AC	6/1/2013	4" DIGOUTS	6/14/2021	60
NHAROL	007	NORTH HAROLD ST. 005	ALDER ST	N.F. CURB OAK ST	2	Collector	400	52	20,800	AC	8/1/2011	MICROSURFACING/RU BB CHIP SEAL/SLURRY	6/11/2021	60



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Street ID	Section ID	Street Name	Beg Location	End Location	Lanes	Functional Class	Length	Width	Area	Surface Type	Last M&R Date	Last M&R Treatment	Inspection Date	PCI
SOUTH	015	SOUTH ST. 015	END OF GD. PAVEMENT 0+00 @001	S.EAST P.L. ROSSI RES. 4+30	2	Residential/Local	430	22	9,460	GRAVEL			-	-
TEMLN	001	TEMPLE LN	FORT-BRAGG SHERWOOD RD	NORTH END	2	Residential/Local	350	18	6,300	GRAVEL			-	-
WINNIF	001	WINNIFRED ST.	W. EDGE OF PAVEMENT 0+00	E.F. CURB BRANDON WAY 8+42	2	Residential/Local	842	34	28,628	AC			6/11/2021	0

**ALLEYWAYS**

City of Fort Bragg  
PCI List by Descending PCI (Alleyways)  
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Street ID	Section ID	Alleyways Name	Beg Location	End Location	Lanes	Functional Class	Length	Width	Area	Surface Type	PCI/M&R Date	PCI
N200E	N200E1	N200E	E ALDER ST	E REDWOOD AVE	2	Other (Alleyways)	418	12	5016	PCC	8/2/2021	100
N300E	N300E1	N300E	E REDWOOD AVE	E LAUREL ST	2	Other (Alleyways)	416	18	7488	AC	7/31/2021	100
N800C	N800C1	N800C	W SPRUCE ST	W ELM ST	2	Other (Alleyways)	422	16	6752	AC	8/4/2021	100
S100G	S100G1	S100G	MADRONE ST	E OAK ST	2	Other (Alleyways)	418	10	4180	PCC	8/2/2021	100
N102G	N102G1	N102G	300 N/O E OAK ST	E ALDER ST	2	Other (Alleyways)	117	16	1872	AC	12/10/2021	100
S600E	S600E1	S600E	E END	WALNUT ST	2	Other (Alleyways)	610	12	7320	AC	12/10/2021	100
S100H	S100H1	S100H	MADRONE ST	E OAK ST	2	Other (Alleyways)	410	14	5740	AC	12/10/2021	100
S300G	S300G1	S300G	HAZEL ST	MAPLE ST	2	Other (Alleyways)	373	16	5968	AC	12/10/2021	100
S300K	S300K1	S300K	S END	MAPLE ST	2	Other (Alleyways)	579	13	7527	AC	12/10/2021	100
S100J	S100J1	S100J	MADRONE ST	E OAK ST	2	Other (Alleyways)	418	14	5852	AC	8/2/2021	98
S302N	S302N1	S302N	S301N	WALL ST	2	Other (Alleyways)	139	16	2224	AC	8/4/2021	98
S500D	S500D1	S500D	WALNUT ST	E CHESTNUT ST	2	Other (Alleyways)	614	16	9824	AC	8/2/2021	98
S500H	S500H1	S500H	WALNUT ST	E CHESTNUT ST	2	Other (Alleyways)	635	18	11430	AC	8/2/2021	98
S301N	S301N1	S301N	S302N	WILLOW ST	2	Other (Alleyways)	652	12	7824	AC	8/3/2021	96
N600J	N600J1	N600J	E FIR ST	N END	2	Other (Alleyways)	662	14	9268	AC	8/4/2021	39
S201F	S201F1	S201F	105' N/O MAPLE ST	MADRONE ST	2	Other (Alleyways)	309	12	3708	AC	8/3/2021	34
N600C	N600C1	N600C	W FIR ST	W BUSH ST	2	Other (Alleyways)	424	16	6784	AC	8/2/2021	30
S400K	S400K1	S400K	E CHESTNUT ST	N END	2	Other (Alleyways)	276	20	5520	AC	8/4/2021	24
N100D	N100D1	N100D	E OAK ST	E ALDER ST	2	Other (Alleyways)	417	20	8340	AC	7/31/2021	23
N200D	N200D1	N200D	E ALDER ST	E REDWOOD AVE	2	Other (Alleyways)	405	18	7290	AC	7/31/2021	22
N401G	N401G1	N401G	214' N/O LAUREL ST	E PINE ST	2	Other (Alleyways)	206	14	2884	AC	7/31/2021	22
N800D	N800D1	N800D	E SPRUCE ST	E ELM ST	2	Other (Alleyways)	423	18	7614	AC	8/2/2021	21
S300E	S300E1	S300E	HAZEL ST	MAPLE ST	2	Other (Alleyways)	356	16	5696	AC	8/3/2021	20
N101C	N101C1	N101C	W OAK ST	W ALDER ST	2	Other (Alleyways)	420	16	6720	AC	7/31/2021	19
N102C	N102C1	N102C	N101C	MAIN ST	2	Other (Alleyways)	146	26	3796	AC	7/31/2021	18
S200D	S200D1	S200D	MAPLE ST	MADRONE ST	2	Other (Alleyways)	425	16	6800	AC	8/3/2021	16
S200G	S200G1	S200G	MAPLE ST	MADRONE ST	2	Other (Alleyways)	425	14	5950	AC	8/3/2021	16
N200C	N200C1	N200C	W ALDER ST	W REDWOOD AVE	2	Other (Alleyways)	439	17	7463	AC	8/2/2021	11
N600G	N600G1	N600G	E FIR ST	E BUSH ST	2	Other (Alleyways)	422	17	7174	AC	8/2/2021	11
S300D	S300D1	S300D	HAZEL ST	MAPLE ST	2	Other (Alleyways)	358	16	5728	AC	8/3/2021	10
N100E	N100E1	N100E	E OAK ST	E ALDER ST	2	Other (Alleyways)	413	16	6608	AC	7/31/2021	8
N300D	N300D1	N300D	E REDWOOD AVE	E LAUREL ST	2	Other (Alleyways)	413	18	7434	AC	7/31/2021	8
N300J	N300J1	N300J	E REDWOOD AVE	E LAUREL ST	2	Other (Alleyways)	419	16	6704	AC	7/31/2021	8
N500D	N500D1	N500D	E PINE ST	E FIR ST	2	Other (Alleyways)	407	18	7326	AC	8/2/2021	8
N900D	N900D1	N900D	E ELM ST	E MANZANITA ST	2	Other (Alleyways)	405	20	8100	AC	8/2/2021	8
N200F	N200F1	N200F	E ALDER ST	E REDWOOD AVE	2	Other (Alleyways)	407	14	5698	AC	7/31/2021	6
S100M	S100M1	S100M	WILLOW ST	E OAK ST	2	Other (Alleyways)	603	14	8442	AC	8/3/2021	6
S300P	S300P1	S300P	WALL ST	WILLOW ST	2	Other (Alleyways)	955	20	19100	AC	8/3/2021	6
N600B	N600B1	N600B	W FIR ST	W BUSH ST	2	Other (Alleyways)	416	16	6656	AC	8/2/2021	5
N100H	N100H1	N100H	E OAK ST	E ALDER ST	2	Other (Alleyways)	414	16	6624	AC	7/31/2021	4
N200H	N200H1	N200H	E ALDER ST	E REDWOOD AVE	2	Other (Alleyways)	424	14	5936	AC	7/31/2021	4
N300H	N300H1	N300H	E REDWOOD AVE	E LAUREL ST	2	Other (Alleyways)	426	16	6816	AC	7/31/2021	4
N400H	N400H1	N400H	E LAUREL ST	E PINE ST	2	Other (Alleyways)	414	16	6624	AC	7/31/2021	4
N500G	N500G1	N500G	E PINE ST	E FIR ST	2	Other (Alleyways)	428	16	6848	AC	8/2/2021	4
S100K	S100K1	S100K	WILLOW ST	E OAK ST	2	Other (Alleyways)	606	14	8484	AC	8/3/2021	4
S300J	S300J1	S300J	S END	MAPLE ST	2	Other (Alleyways)	356	14	4984	AC	8/3/2021	4
S800E	S800E1	S800E	N HARBOR DR	SOUTH ST	2	Other (Alleyways)	592	16	9472	AC	8/2/2021	4
N400J	N400J1	N400J	E LAUREL ST	E PINE ST	2	Other (Alleyways)	424	16	6784	AC	7/31/2021	3
N500J	N500J1	N500J	E PINE ST	E FIR ST	2	Other (Alleyways)	427	18	7686	AC	7/31/2021	3

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Street ID	Section ID	Alleyways Name	Beg Location	End Location	Lanes	Functional Class	Length	Width	Area	Surface Type	PCI/M&R Date	PCI
S202F	S202F1	S202F	MAPLE ST	105' N/O MAPLE ST	2	Other (Alleyways)	105	14	1470	AC	8/3/2021	3
N400E	N400E1	N400E	E LAUREL ST	E PINE ST	1	Other (Alleyways)	426	18	7668	AC	7/31/2021	1
N402G	N402G1	N402G	E LAUREL ST	214' N/O LAUREL ST	2	Other (Alleyways)	214	14	2996	AC	8/4/2021	1
S100E	S100E1	S100E	MADRONE ST	E OAK ST	2	Other (Alleyways)	417	16	6672	AC	8/3/2021	1
S100P	S100P1	S100P	WILLOW ST	E OAK ST	2	Other (Alleyways)	605	16	9680	AC	8/3/2021	1
S200E	S200E1	S200E	MAPLE ST	MADRONE ST	2	Other (Alleyways)	402	17	6834	AC	8/3/2021	1
N100J	N100J1	N100J	E OAK ST	E ALDER ST	2	Other (Alleyways)	418	16	6688	AC	7/31/2021	0
N200J	N200J1	N200J	E ALDER ST	E REDWOOD AVE	2	Other (Alleyways)	425	16	6800	AC	8/4/2021	0
N300F	N300F1	N300F	E REDWOOD AVE	E LAUREL ST	2	Other (Alleyways)	414	14	5796	AC	7/31/2021	0
N400D	N400D1	N400D	E LAUREL ST	E PINE ST	2	Other (Alleyways)	410	20	8200	AC	7/31/2021	0
N500E	N500E1	N500E	E PINE ST	E FIR ST	2	Other (Alleyways)	418	16	6688	AC	8/2/2021	0
N500F	N500F1	N500F	E PINE ST	E FIR ST	2	Other (Alleyways)	417	16	6672	AC	8/2/2021	0
S100L	S100L1	S100L	WILLOW ST	E OAK ST	2	Other (Alleyways)	608	14	8512	AC	8/3/2021	0
S200L	S200L1	S200L	MAPLE ST	WILLOW ST	2	Other (Alleyways)	242	14	3388	AC	8/3/2021	0
S400H	S400H1	S400H	E CHESTNUT ST	N END	2	Other (Alleyways)	641	14	8974	AC	8/3/2021	0
S100D	S100D1	S100D	S END	E OAK ST	2	Other (Alleyways)	20	16	320	AC	-	-
N100F	N100F1	N100F	E OAK ST	E ALDER ST	2	Other (Alleyways)	413	14	5782	GRAVEL	-	-
N100K	N100K1	N100K	E OAK ST	E ALDER ST	2	Other (Alleyways)	420	16	6720	GRAVEL	-	-
N100L	N100L1	N100L	E OAK ST	E ALDER ST	2	Other (Alleyways)	428	14	5992	GRAVEL	-	-
N101G	N101G1	N101G	E OAK ST	300' N/O E OAK ST	2	Other (Alleyways)	300	16	4800	GRAVEL	-	-
N200G	N200G1	N200G	E ALDER ST	E REDWOOD AVE	2	Other (Alleyways)	423	14	5922	GRAVEL	-	-
N200K	N200K1	N200K	CEDAR ST	E REDWOOD AVE	2	Other (Alleyways)	194	14	2716	GRAVEL	-	-
N200L	N200L1	N200L	MORROW ST	E END	2	Other (Alleyways)	200	12	2400	GRAVEL	-	-
N300G	N300G1	N300G	E REDWOOD AVE	N END	2	Other (Alleyways)	168	14	2352	GRAVEL	-	-
N300K	N300K1	N300K	E REDWOOD AVE	E LAUREL ST	2	Other (Alleyways)	431	16	6896	GRAVEL	-	-
N400F	N400F1	N400F	E LAUREL ST	E PINE ST	2	Other (Alleyways)	426	16	6816	GRAVEL	-	-
N500A	N500A1	N500A	W PINE ST	W FIR ST	2	Other (Alleyways)	426	18	7668	GRAVEL	-	-
N500B	N500B1	N500B	W PINE ST	W FIR ST	2	Other (Alleyways)	427	16	6832	GRAVEL	-	-
N500H	N500H1	N500H	E PINE ST	E FIR ST	2	Other (Alleyways)	422	12	5064	GRAVEL	-	-
N600A	N600A1	N600A	W FIR ST	W BUSH ST	2	Other (Alleyways)	460	16	7360	GRAVEL	-	-
N600F	N600F1	N600F	E FIR ST	E BUSH ST	2	Other (Alleyways)	417	16	6672	GRAVEL	-	-
N600H	N600H1	N600H	E FIR ST	E BUSH ST	2	Other (Alleyways)	426	14	5964	GRAVEL	-	-
N700A	N700A1	N700A	W BUSH ST	W SPRUCE ST	2	Other (Alleyways)	458	16	7328	GRAVEL	-	-
N700B	N700B1	N700B	W BUSH ST	W SPRUCE ST	2	Other (Alleyways)	425	14	5950	GRAVEL	-	-
N700C	N700C1	N700C	W BUSH ST	W SPRUCE ST	2	Other (Alleyways)	409	14	5726	GRAVEL	-	-
N700G	N700G1	N700G	E BUSH ST	WINIFRED ST	2	Other (Alleyways)	268	14	3752	GRAVEL	-	-
N700H	N700H1	N700H	E BUSH ST	WINIFRED ST	2	Other (Alleyways)	264	18	4752	GRAVEL	-	-
N800G	N800G1	N800G	WINIFRED ST	N END	2	Other (Alleyways)	358	14	5012	GRAVEL	-	-
S100F	S100F1	S100F	MADRONE ST	E OAK ST	2	Other (Alleyways)	418	18	7524	GRAVEL	-	-
S100N	S100N1	S100N	WILLOW ST	E OAK ST	2	Other (Alleyways)	616	16	9856	GRAVEL	-	-
S200H	S200H1	S200H	MAPLE ST	MADRONE ST	2	Other (Alleyways)	410	14	5740	GRAVEL	-	-
S200K	S200K1	S200K	MAPLE ST	WILLOW ST	2	Other (Alleyways)	258	16	4128	GRAVEL	-	-
S300F	S300F1	S300F	HAZEL ST	MAPLE ST	2	Other (Alleyways)	363	10	3630	GRAVEL	-	-
S300H	S300H1	S300H	S END	MAPLE ST	2	Other (Alleyways)	364	15	5460	GRAVEL	-	-
S500J	S500J1	S500J	WALNUT ST	E CHESTNUT ST	2	Other (Alleyways)	378	18	6804	GRAVEL	-	-

## Appendix B

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### MAINTENANCE AND REHABILITATION DECISION TREE



## Maintenance and Rehabilitation (M&R) Decision Tree

This report presents the current maintenance and rehabilitation decision tree that exists in the database. The decision tree forms the basis for all of the budgetary computations included in this report. **Changes to the decision tree will make the results in the budget reports invalid.** All pavement treatment unit costs relevant to the street types in the database were updated.

The decision tree lists the treatments and costs selected for preventive maintenance and rehabilitation activities. Each line represents a specific combination of functional classification and surface type.

The preventive maintenance portion of the report is identified as Condition Category I – Very Good. All preventive maintenance treatment listings are assigned only to sections in Condition Category I where the PCI  $\geq$  70. Sections with PCI values less than 70 are assigned to treatments listed in Categories II through V.

In the preventive maintenance category (PCI  $\geq$  70), a time sequence is used to identify the appropriate treatment and cost. Each preventive maintenance treatment description consists of three parts: 1) a CRACK treatment, 2) a SURFACE treatment, and 3) a RESTORATION treatment. These three parts allow the user to specify one of three different preventive maintenance treatments depending on the prior maintenance history of the section.

1. The CRACK treatment part can be used to specify the most frequent type of preventive maintenance activity planned (typically crack seals).
2. The SURFACE treatment part can be used to specify more extensive and less frequent preventive maintenance activities, such as chip seals or slurry seals. For example, a crack seal can be specified on a 3-year cycle with a slurry seal specified after 5 years.
3. The RESTORATION part can be used to specify a surface restoration treatment (such as an overlay) to be performed after a specified number of surface treatments. For example, after a certain number of successive slurry seals, an overlay can be specified instead of another slurry seal.

Rehabilitation treatments are assigned to sections in Condition Categories II through V (PCI less than 70). Each line is defined by a specific combination of functional classification, surface type, and condition category.

COLUMN	DESCRIPTION
Functional Class	Functional Classification identifying the branch
Surface	Surface Type identifying the branch number.
Condition Category	Condition Category (I through V).
Treatment Type	First Row (Crack Treatment) indicates localized treatment (e.g. crack sealing). Second Row (Surface Treatment) indicates surface treatment

COLUMN	DESCRIPTION
	(e.g. slurry sealing). Third Row (Restoration Treatment) indicates surface restoration (e.g. overlay).
Treatment	Name of treatments from the "Treatment Descriptions" report.
Yrs. Between Crack Seals	First Row - number of years between successive treatment applications specified in the first row (i.e. CRACK treatment).
Yrs. Between Surface Seals	Second Row - number of years between successive treatment applications specified in the second row (i.e. SURFACE treatment).
Number of Sequential Seals	Number of times that the treatment application in the second row (i.e. SURFACE treatment) will be performed prior to performing the treatment application in the third row.

Note that the treatments assigned to each section should not be blindly followed in preparing a street maintenance program. Engineering judgment and project level analysis should be applied to ensure that the treatment is appropriate and cost effective for the section.

# Decision Tree

Printed: 11/23/2021



Functional Class	Surface	Condition Category	Treatment Type	Treatment	Cost/Sq Yd, except Seal Cracks in LF:	Yrs Between Crack Seals	Yrs Between Surface Seals	# of Surface Seals before Overlay
Arterial	AC	I - Very Good	Crack Treatment	SEAL CRACKS	\$1.00	3		
			Surface Treatment	SLURRY SEAL W/ CRACK SEAL	\$6.50		5	
			Restoration Treatment	DO NOTHING	\$0.00			100
		II - Good, Non-Load Related		RUBBERIZED CHIP SEAL+MICROSURFACING W/ 3% DIGOUTS	\$14.50		5	
		III - Good, Load Related		AC OVERLAY 2 INCHES	\$52.25			
		IV - Poor		THICK MILL AND OVERLAY	\$118.25			
		V - Very Poor		12" FDR W/ 4" HMA	\$144.75			
	AC/AC	I - Very Good	Crack Treatment	SEAL CRACKS	\$1.00	3		
			Surface Treatment	SLURRY SEAL W/ CRACK SEAL	\$6.50		5	
			Restoration Treatment	DO NOTHING	\$0.00			100
		II - Good, Non-Load Related		RUBBERIZED CHIP SEAL+MICROSURFACING W/ 3% DIGOUTS	\$14.50		5	
		III - Good, Load Related		AC OVERLAY 2 INCHES	\$52.25			
		IV - Poor		THICK MILL AND OVERLAY	\$118.25			
		V - Very Poor		12" FDR W/ 4" HMA	\$144.75			
	AC/PCC	I - Very Good	Crack Treatment	DO NOTHING	\$0.00	99		
			Surface Treatment	DO NOTHING	\$0.00		3	
			Restoration Treatment	LC & THICK OL W/ FABRIC	\$8.82			1
		II - Good, Non-Load Related		THIN AC OVERLAY(1.5 INCHES)	\$5.76			
		III - Good, Load Related		MILL & THIN OL W/FABRIC	\$10.08			
		IV - Poor		LC & THICK OL W/ FABRIC	\$8.82			
		V - Very Poor		RECONSTRUCT SURFACE (AC)	\$14.00			

Functional Class and Surface combination not used  
 Selected Treatment is not a Surface Seal

# Decision Tree

Printed: 11/23/2021



Functional Class	Surface	Condition Category	Treatment Type	Treatment	Cost/Sq Yd, except Seal Cracks in LF:	Yrs Between Crack Seals	Yrs Between Surface Seals	# of Surface Seals before Overlay
Collector	AC	I - Very Good	Crack Treatment	SEAL CRACKS	\$1.00	3		
			Surface Treatment	SLURRY SEAL W/ CRACK SEAL	\$6.00		5	
			Restoration Treatment	DO NOTHING	\$0.00			100
		II - Good, Non-Load Related		RUBBERIZED CHIP SEAL+MICROSURFACING W/ 3% DIGOUTS	\$13.75		6	
		III - Good, Load Related		AC OVERLAY 2 INCHES	\$49.75			
		IV - Poor		THICK MILL AND OVERLAY	\$112.50			
		V - Very Poor		8" FDR W/ 3.5" HMA	\$117.75			
	AC/AC	I - Very Good	Crack Treatment	SEAL CRACKS	\$1.00	3		
			Surface Treatment	SLURRY SEAL W/ CRACK SEAL	\$6.00		5	
			Restoration Treatment	DO NOTHING	\$0.00			100
		II - Good, Non-Load Related		RUBBERIZED CHIP SEAL+MICROSURFACING W/ 3% DIGOUTS	\$13.75		6	
		III - Good, Load Related		AC OVERLAY 2 INCHES	\$49.75			
		IV - Poor		THICK MILL AND OVERLAY	\$112.50			
		V - Very Poor		8" FDR W/ 3.5" HMA	\$118.00			
	AC/PCC	I - Very Good	Crack Treatment	SEAL CRACKS	\$0.60	3		
			Surface Treatment	SLURRY SEAL	\$0.78		4	
			Restoration Treatment	LC & THICK OL W/ FABRIC	\$8.82			1
		II - Good, Non-Load Related		THIN AC OVERLAY(1.5 INCHES)	\$5.76			
		III - Good, Load Related		MILL & THIN OL W/FABRIC	\$10.08			
		IV - Poor		LC & THICK OL W/ FABRIC	\$8.82			
		V - Very Poor		RECONSTRUCT SURFACE (AC)	\$11.38			

 Functional Class and Surface combination not used  
 Selected Treatment is not a Surface Seal

# Decision Tree

Printed: 11/23/2021

Functional Class	Surface	Condition Category	Treatment Type	Treatment	Cost/Sq Yd, except Seal Cracks in LF:	Yrs Between Crack Seals	Yrs Between Surface Seals	# of Surface Seals before Overlay
Residential/Local	AC	I - Very Good	Crack Treatment	SEAL CRACKS	\$1.00	4		
			Surface Treatment	SLURRY SEAL W/ CRACK SEAL	\$5.50		7	
			Restoration Treatment	DO NOTHING	\$0.00			100
		II - Good, Non-Load Related		MICROSURFACING W/ CRACK SEAL AND 3% DIGOUTS	\$8.00		7	
		III - Good, Load Related		MICROSURFACING W/ CRACK SEAL AND 5% DIGOUTS	\$11.50		7	
		IV - Poor		THICK MILL AND OVERLAY	\$89.00			
		V - Very Poor		4.5" SURFACE RECONSTRUCT	\$114.25			
	AC/AC	I - Very Good	Crack Treatment	SEAL CRACKS	\$1.00	4		
			Surface Treatment	SLURRY SEAL W/ CRACK SEAL	\$5.50		5	
			Restoration Treatment	DO NOTHING	\$0.00			100
		II - Good, Non-Load Related		MICROSURFACING W/ CRACK SEAL AND 3% DIGOUTS	\$8.00		7	
		III - Good, Load Related		MICROSURFACING W/ CRACK SEAL AND 5% DIGOUTS	\$11.50		7	
		IV - Poor		THICK MILL AND OVERLAY	\$89.00			
		V - Very Poor		4.5" SURFACE RECONSTRUCT	\$114.25			
	AC/PCC	I - Very Good	Crack Treatment	SEAL CRACKS	\$0.60	3		
			Surface Treatment	SLURRY SEAL	\$0.78		4	
			Restoration Treatment	LC & THICK OL W/ FABRIC	\$8.82			1
		II - Good, Non-Load Related		DO NOTHING	\$0.00			
		III - Good, Load Related		THIN OVERLAY w/FABRIC	\$7.02			
		IV - Poor		LC & THICK OL W/ FABRIC	\$8.82			
		V - Very Poor		RECONSTRUCT SURFACE (AC)	\$8.75			

 Functional Class and Surface combination not used  
 Selected Treatment is not a Surface Seal

# Decision Tree

Printed: 11/23/2021

Functional Class	Surface	Condition Category	Treatment Type	Treatment	Cost/Sq Yd, except Seal Cracks in LF:	Yrs Between Crack Seals	Yrs Between Surface Seals	# of Surface Seals before Overlay
Residential/Local	PCC	I - Very Good	Crack Treatment	DO NOTHING	\$0.00	99		
			Surface Treatment	DO NOTHING	\$0.00		99	
			Restoration Treatment	DO NOTHING	\$0.00			100
		II - Good, Non-Load Related		DO NOTHING	\$0.00			
		III - Good, Load Related		DO NOTHING	\$0.00			
		IV - Poor		THICK OVERLAY W/ FABRIC (2")	\$8.12			
		V - Very Poor		PULVERIZE,RESHAPE,COMPACT	\$9.45			
	ST	I - Very Good	Crack Treatment	DO NOTHING	\$0.00	99		
			Surface Treatment	DO NOTHING	\$0.00		99	
			Restoration Treatment	DO NOTHING	\$0.00			100
		II - Good, Non-Load Related		DO NOTHING	\$0.00			
		III - Good, Load Related		RUBBERIZED CHIP SEAL	\$5.00			
		IV - Poor		RUBBERIZED CHIP SEAL	\$5.00			
		V - Very Poor		AC OVERLAY 1.5 INCHES	\$39.75			

- Functional Class and Surface combination not used
- Selected Treatment is not a Surface Seal

# Decision Tree

Printed: 11/23/2021

Functional Class	Surface	Condition Category	Treatment Type	Treatment	Cost/Sq Yd, except Seal Cracks in LF:	Yrs Between Crack Seals	Yrs Between Surface Seals	# of Surface Seals before Overlay
Other	AC	I - Very Good	Crack Treatment	SEAL CRACKS	\$1.60	4		
			Surface Treatment	SINGLE CHIP SEAL	\$3.00		8	
			Restoration Treatment	MILL AND THIN OVERLAY	\$20.25			3
		II - Good, Non-Load Related		SINGLE CHIP SEAL	\$3.00		8	
		III - Good, Load Related		RUBB CHIP SEAL + MICROSURFACING	\$7.50			
		IV - Poor		AC OVERLAY 1.5 INCHES	\$22.25			
		V - Very Poor		THICK MILL AND OVERLAY	\$36.75			
	AC/AC	I - Very Good	Crack Treatment	SEAL CRACKS	\$1.60	4		
			Surface Treatment	SINGLE CHIP SEAL	\$1.74		8	
			Restoration Treatment	MILL AND THIN OVERLAY	\$15.04			3
		II - Good, Non-Load Related		DOUBLE CHIP SEAL	\$1.52			
		III - Good, Load Related		HEATER SCARIFY & OVERLAY	\$5.95			
		IV - Poor		HEATER SCARIFY & OVERLAY	\$6.14			
		V - Very Poor		RECONSTRUCT SURFACE (AC)	\$8.75			
	AC/PCC	I - Very Good	Crack Treatment	SEAL CRACKS	\$1.60	4		
			Surface Treatment	SINGLE CHIP SEAL	\$1.74		8	
			Restoration Treatment	MILL AND THIN OVERLAY	\$15.04			3
		II - Good, Non-Load Related		DOUBLE CHIP SEAL	\$1.52			
		III - Good, Load Related		HEATER SCARIFY & OVERLAY	\$5.95			
		IV - Poor		HEATER SCARIFY & OVERLAY	\$6.14			
		V - Very Poor		RECONSTRUCT SURFACE (AC)	\$8.75			

- Functional Class and Surface combination not used
- Selected Treatment is not a Surface Seal

# Decision Tree

Printed: 11/23/2021

Functional Class	Surface	Condition Category	Treatment Type	Treatment	Cost/Sq Yd, except Seal Cracks in LF:	Yrs Between Crack Seals	Yrs Between Surface Seals	# of Surface Seals before Overlay
Other	PCC	I - Very Good	Crack Treatment	DO NOTHING	\$0.00	99		
			Surface Treatment	DO NOTHING	\$0.00		99	
			Restoration Treatment	DO NOTHING	\$0.00			100
		II - Good, Non-Load Related		DO NOTHING	\$0.00			
		III - Good, Load Related		DO NOTHING	\$0.00			
		IV - Poor		THICK AC OVERLAY(2.5 INCHES)	\$12.25			
		V - Very Poor		THICK AC OVERLAY(2 INCHES)	\$23.25			
	ST	I - Very Good	Crack Treatment	DO NOTHING	\$0.00	9		
			Surface Treatment	DO NOTHING	\$0.00		99	
			Restoration Treatment	THICK AC OVERLAY(2.5 INCHES)	\$0.00			100
		II - Good, Non-Load Related		SINGLE CHIP SEAL	\$1.11			
		III - Good, Load Related		SINGLE CHIP SEAL	\$1.51			
		IV - Poor		SINGLE CHIP SEAL	\$1.92			
		V - Very Poor		THICK AC OVERLAY(2.5 INCHES)	\$7.27			

- Functional Class and Surface combination not used
- Selected Treatment is not a Surface Seal



## Appendix C

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### BUDGET NEEDS ANALYSIS RESULTS

## Budget Needs Reports

The purpose of this section is to answer the question: ***If the agency had all the money in the world, what sections should be fixed and how much will it cost?*** Based on the Maintenance & Rehabilitation (M&R) decision tree and the PCIs of the sections, the program will then select a maintenance or rehabilitation action and compute the total costs over the analysis period. The Budget Needs represents the "ideal world" funding levels, while the Budget Scenario reports in the next section represent the most "cost effective" prioritization possible for the actual funding levels.

A budget needs analysis has been performed. The summary results from the analysis are provided. An interest rate of 3% and an inflation factor of 3% were used to project the costs for the analysis period. This report shows the total ten-year budget that would be required to meet the agency's standards as exemplified in the M&R decision tree.

Budget Needs reports included in this appendix are listed below:

- Projected PCI/Cost Summary
- Preventive Maintenance Treatment/Cost Summary
- Rehabilitation Treatment/Cost Summary

## Needs - Projected PCI /Cost Summary

This report summarizes and projects the network PCI over the ten-year analysis period, both with and without treatments applied. It also reports the associated costs, which are based on the treatment unit costs presented in the M&R decision tree.

<b>COLUMN</b>	<b>DESCRIPTION</b>
Year	Year in the analysis period.
PCI Treated	Projected network average PCI with all needed treatments applied.
PCI Untreated	Projected network average PCI without any treatments applied.
PM Cost	Total preventive maintenance treatment cost.
Rehab Cost	Total rehabilitation treatment cost.
Cost	The budget required for each year in the analysis period to meet the agency's standard as shown on the M&R decision tree.
Total Cost	Total budget required over a ten-year period.

# Needs - Projected PCI/Cost Summary

Inflation Rate = 3.00 % Printed: 1/28/2022

Year	PCI Treated	PCI Untreated	PM Cost	Rehab Cost	Cost
2022	92	65	\$1,575,592	\$17,974,956	\$19,550,548
2023	88	62	\$0	\$264,548	\$264,548
2024	87	60	\$112,026	\$765,423	\$877,449
2025	85	58	\$4,043	\$0	\$4,043
2026	84	56	\$26,831	\$0	\$26,831
2027	86	54	\$1,977,082	\$0	\$1,977,082
2028	84	51	\$37,730	\$33,873	\$71,603
2029	85	49	\$1,390,490	\$632,743	\$2,023,233
2030	85	47	\$111,548	\$85,717	\$197,265
2031	84	45	\$66,494	\$263,626	\$330,120
		% PM	PM Total Cost	Rehab Total Cost	Total Cost
		20.94%	\$5,301,836	\$20,020,886	\$25,322,722

## Needs - Preventive Maintenance Treatment/Cost Summary

This report summarizes each preventive maintenance treatment type, quantity of pavement affected, and total costs over the analysis period. It also summarizes the total quantities and costs over the next ten years.

<b>COLUMN</b>	<b>DESCRIPTION</b>
Treatment	Type of preventive maintenance treatments needed.
Year	Year in the analysis period (i.e. 2022, 2023, 2024, etc.).
Area Treated	Quantities in linear feet (Seal Cracks) or square yard (Slurry Seal).
Cost	Maintenance treatment cost.

## Needs - Preventive Maintenance Treatment/Cost Summary

Inflation Rate = 3.00 %      Printed: 1/28/2022

Treatment	Year	Area Treated	Cost
SEAL CRACKS	2022	53.19 sq. yd.	\$55
	2024	4.01 sq. yd.	\$5
	2025	3,655.1 sq. yd.	\$4,043
	2026	4,364.96 sq. yd.	\$5,221
	2027	416.73 sq. yd.	\$485
	2028	615.54 sq. yd.	\$743
	2029	13.21 sq. yd.	\$17
	2030	4,359.71 sq. yd.	\$5,546
	2031	503.02 sq. yd.	\$671
	Total		13,985.48
SINGLE CHIP SEAL	2022	5,710.44 sq. yd.	\$17,133
	2029	3,158.56 sq. yd.	\$10,074
	2030	43,943.67 sq. yd.	\$106,002
Total		52,812.67	\$133,209
SLURRY SEAL W/ CRACK SEAL	2022	271,209.56 sq. yd.	\$1,558,404
	2024	19,112.22 sq. yd.	\$112,021
	2026	3,200 sq. yd.	\$21,610
	2027	280,973.56 sq. yd.	\$1,976,597
	2028	5,162.67 sq. yd.	\$36,987
	2029	202,975.78 sq. yd.	\$1,380,399
	2031	8,881.33 sq. yd.	\$65,823
Total		791,515.11	\$5,151,841
Total Quantity		858,313.25	\$5,301,836

## Needs - Rehabilitation Treatment/Cost Summary

This report summarizes each rehabilitation treatment type, quantity of pavement affected, and total costs over the analysis period. It also summarizes the total quantities and costs over the next ten years.

<b>COLUMN</b>	<b>DESCRIPTION</b>
Treatment	Type of rehabilitation treatments needed.
Year	Year in the analysis period (i.e. 2022, 2023, 2024, etc.).
Area Treated	Quantities in square yard.
Cost	Rehabilitation treatment cost.

# Needs - Rehabilitation Treatment/Cost Summary

Inflation Rate = 3.00 % Printed: 1/28/2022

Treatment	Year	Area Treated	Cost
12" FDR W/ 4" HMA	2022	3,005 sq.yd.	\$434,974
	<b>Total</b>	<b>3,005 sq.yd.</b>	<b>\$434,974</b>
4.5" SURFACE RECONSTRUCT	2022	33,327.33 sq.yd.	\$3,807,654
	<b>Total</b>	<b>33,327.33 sq.yd.</b>	<b>\$3,807,654</b>
8" FDR W/ 3.5" HMA	2022	14,446.11 sq.yd.	\$1,704,642
	<b>Total</b>	<b>14,446.11 sq.yd.</b>	<b>\$1,704,642</b>
MICROSURFACING W/ CRACK SEAL AND 3% DIGOUTS	2022	9,702 sq.yd.	\$77,618
	2029	7,131.78 sq.yd.	\$70,172
	<b>Total</b>	<b>16,833.78 sq.yd.</b>	<b>\$147,790</b>
MICROSURFACING W/ CRACK SEAL AND 5% DIGOUTS	2022	24,677.11 sq.yd.	\$283,792
	2024	19,968.67 sq.yd.	\$243,628
	2029	23,468.22 sq.yd.	\$331,928
	2031	17,569.22 sq.yd.	\$263,626
	<b>Total</b>	<b>85,683.22 sq.yd.</b>	<b>\$1,122,974</b>
RUBBERIZED CHIP SEAL+MICROSURFACING W/ 3% DIGOUTS	2022	2,063.11 sq.yd.	\$28,368
	2024	8,387.78 sq.yd.	\$122,357
	2028	2,063.11 sq.yd.	\$33,873
	2030	4,921.11 sq.yd.	\$85,717
	<b>Total</b>	<b>17,435.11 sq.yd.</b>	<b>\$270,315</b>
THICK MILL AND OVERLAY	2022	98,874.44 sq.yd.	\$8,020,155
	2029	2,107.11 sq.yd.	\$230,643
	<b>Total</b>	<b>100,981.56 sq.yd.</b>	<b>\$8,250,798</b>
AC OVERLAY 2 INCHES	2022	70,587.56 sq.yd.	\$3,568,901
	2023	5,162.67 sq.yd.	\$264,548
	2024	7,568 sq.yd.	\$399,438
	<b>Total</b>	<b>83,318.22 sq.yd.</b>	<b>\$4,232,887</b>
AC OVERLAY 1.5 INCHES	2022	2,195.56 sq.yd.	\$48,852
	<b>Total</b>	<b>2,195.56 sq.yd.</b>	<b>\$48,852</b>
<b>Total Cost</b>			<b>\$20,020,886</b>



## Appendix D

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### BUDGET SCENARIO RESULTS

## **Scenario 1: Existing Budget**

Cost Summary Report  
Network Condition Summary Report

# Scenarios - Cost Summary

Interest: 3.00%

Inflation: 3.00%

Printed: 1/28/2022

Scenario: 2021 S1: CITY'S EXISTING FUNDING (\$1.0M/YEAR)

Year	PM	Budget	Rehabilitation	Preventative Maintenance	Surplus PM	Deferred	Stop Gap			
2022	12%	\$1,000,000	II	\$0	Non-Project	\$1,333	\$364	\$18,080,618	Funded	\$0
			III	\$2,594					Unmet	\$85,790
			IV	\$0						
			V	\$0						
			Total Project	\$2,594						
2023	12%	\$1,000,000	II	\$51,262	Non-Project	\$269,232	\$0	\$18,859,427	Funded	\$0
			III	\$325,406					Unmet	\$691
			IV	\$262,218						
			V	\$75,364						
			Total Project	\$714,250						
2024	5%	\$1,000,000	II	\$122,357	Non-Project	\$100,012	\$0	\$19,622,820	Funded	\$0
			III	\$182,760					Unmet	\$2,665
			IV	\$427,519						
			V	\$156,568						
			Total Project	\$889,204						
2025	8%	\$1,000,000	II	\$0	Non-Project	\$131,366	\$0	\$20,202,109	Funded	\$0
			III	\$358,194					Unmet	\$1,184
			IV	\$364,104						
			V	\$126,837						
			Total Project	\$849,135						
2026	8%	\$1,000,000	II	\$129,025	Non-Project	\$246,862	\$0	\$23,108,469	Funded	\$0
			III	\$95,240					Unmet	\$1,937
			IV	\$289,438						
			V	\$200,201						
			Total Project	\$713,904						
2027	2%	\$1,000,000	II	\$0	Non-Project	\$95,731	\$0	\$26,143,720	Funded	\$0
			III	\$306,569					Unmet	\$114,076
			IV	\$384,891						
			V	\$14,183						
			Total Project	\$705,643						
2028	5%	\$1,000,000	II	\$0	Non-Project	\$136,052	\$0	\$26,493,983	Funded	\$0
			III	\$305,496					Unmet	\$801
			IV	\$298,173						
			V	\$211,179						
			Total Project	\$814,848						
2029	2%	\$1,000,000	II	\$0	Non-Project	\$154,676	\$0	\$27,675,691	Funded	\$0
			III	\$3,191					Unmet	\$3,857
			IV	\$469,944						
			V	\$36,611						
			Total Project	\$509,746						

Scenarios Criteria:

Criteria:

Year	PM	Budget	Rehabilitation	Preventative Maintenance	Surplus PM	Deferred	Stop Gap			
2030	5%	\$1,000,000	II	\$97,068	Non-Project	\$87,192	\$0	\$29,108,518	Funded	\$0
			III	\$91,401					Unmet	\$565
			IV	\$0	Project	\$214				
			V	\$700,360						
			Total Project	\$888,829						
2031	5%	\$1,000,000	II	\$123,116	Non-Project	\$187,558	\$0	\$30,264,795	Funded	\$0
			III	\$163,753					Unmet	\$2,246
			IV	\$0	Project	\$0				
			V	\$502,945						
			Total Project	\$789,814						

## Summary

Functional Class	Rehabilitation	Prev. Maint.	Funded Stop Gap	Unmet Stop Gap
Arterial	\$646,277	\$175,327	\$0	\$28,718
Collector	\$2,938,888	\$783,094	\$0	\$49,656
Other	\$388,413	\$42,096	\$0	\$58,589
Residential/Local	\$3,890,245	\$507,285	\$0	\$76,850
<b>Grand Total:</b>	<b>\$7,863,823</b>	<b>\$1,507,802</b>	<b>\$0</b>	<b>\$213,813</b>

# Scenarios - Network Condition Summary

Interest: 3%

Inflation: 3%

Printed: 1/28/2022

Scenario: 2021 S1: CITY'S EXISTING FUNDING (\$1.0M/YEAR)

Year	Budget	PM	Year	Budget	PM	Year	Budget	PM
2022	\$1,000,000	12%	2026	\$1,000,000	8%	2030	\$1,000,000	5%
2023	\$1,000,000	12%	2027	\$1,000,000	2%	2031	\$1,000,000	5%
2024	\$1,000,000	5%	2028	\$1,000,000	5%			
2025	\$1,000,000	8%	2029	\$1,000,000	2%			

## Projected Network Average PCI by Year

Year	Never Treated	With Selected Treatment	Treated Centerline Miles	Treated Lane Miles
2022	65	66	0.91	1.88
2023	62	65	3.15	6.21
2024	60	64	2.46	4.92
2025	58	64	2.18	4.21
2026	56	63	4.20	8.34
2027	54	62	3.83	7.62
2028	51	61	3.71	7.36
2029	49	60	3.39	6.51
2030	47	59	3.70	7.49
2031	45	58	4.35	8.51

## Percent Network Area by Functional Class and Condition Category

Condition in base year 2022, prior to applying treatments.

Condition	Arterial	Collector	Res/Loc	Other	Total
I	2.9%	20.5%	30.7%	1.6%	55.7%
II / III	3.8%	8.2%	7.2%	0.0%	19.2%
IV	5.5%	1.0%	4.0%	0.4%	10.7%
V	0.5%	2.4%	5.5%	6.0%	14.3%
Total	12.6%	32.1%	47.4%	8.0%	100.0%

Condition in year 2022 after schedulable treatments applied.

Condition	Arterial	Collector	Res/Loc	Other	Total
I	3.8%	21.2%	30.9%	1.6%	57.6%
II / III	3.8%	8.2%	7.1%	0.0%	19.1%
IV	4.9%	1.0%	4.0%	0.4%	10.2%
V	0.1%	1.7%	5.3%	6.0%	13.1%
Total	12.6%	32.1%	47.4%	8.0%	100.0%

Condition in year 2031 after schedulable treatments applied.

Condition	Arterial	Collector	Res/Loc	Other	Total
I	3.9%	16.5%	37.3%	3.2%	60.9%
II / III	0.0%	5.4%	0.6%	0.0%	6.0%
IV	3.8%	5.9%	2.8%	0.0%	12.5%
V	4.9%	4.3%	6.7%	4.8%	20.7%
Total	12.6%	32.1%	47.4%	8.0%	100.0%

## **Scenario 2: Improve PCI to 75**

Cost Summary Report  
Network Condition Summary Report

# Scenarios - Cost Summary

Interest: 3.00%

Inflation: 3.00%

Printed: 1/28/2022

Scenario: 2021 S2 IMPROVE PCI TO 75

Year	PM	Budget	Rehabilitation	Preventative Maintenance	Surplus PM	Deferred	Stop Gap			
2022	0%	\$950,000	II	\$22,336	Non-Project	\$2,006	\$0	\$18,120,902	Funded	\$0
			III	\$2,594					Unmet	\$84,854
			IV	\$0						
			V	\$26,983						
			Total	\$51,913						
		Project	\$895,579							
2023	50%	\$1,300,000	II	\$0	Non-Project	\$652,552	\$0	\$18,491,595	Funded	\$0
			III	\$0					Unmet	\$691
			IV	\$0						
			V	\$647,276						
			Total	\$647,276						
		Project	\$0							
2024	10%	\$850,000	II	\$0	Non-Project	\$98,827	\$0	\$19,383,585	Funded	\$0
			III	\$11,632					Unmet	\$5,327
			IV	\$0						
			V	\$739,128						
			Total	\$750,760						
		Project	\$0							
2025	20%	\$1,100,000	II	\$52,087	Non-Project	\$220,482	\$0	\$19,836,823	Funded	\$0
			III	\$810,260					Unmet	\$1,410
			IV	\$0						
			V	\$16,545						
			Total	\$878,892						
		Project	\$0							
2026	0%	\$3,400,000	II	\$205,183	Non-Project	\$2,605	\$0	\$19,563,309	Funded	\$0
			III	\$653,988					Unmet	\$0
			IV	\$2,340,961						
			V	\$192,986						
			Total	\$3,393,118						
		Project	\$0							
2027	0%	\$4,700,000	II	\$0	Non-Project	\$19,207	\$0	\$17,587,750	Funded	\$0
			III	\$0					Unmet	\$69,346
			IV	\$2,498,184						
			V	\$2,109,057						
			Total	\$4,607,241						
		Project	\$0							
2028	10%	\$4,100,000	II	\$0	Non-Project	\$421,480	\$0	\$15,419,562	Funded	\$0
			III	\$0					Unmet	\$801
			IV	\$3,356,993						
			V	\$319,722						
			Total	\$3,676,715						
		Project	\$0							
2029	0%	\$3,399,996	II	\$11,020	Non-Project	\$73,059	\$0	\$13,702,761	Funded	\$0
			III	\$3,191					Unmet	\$0
			IV	\$2,076,276						
			V	\$1,212,533						
			Total	\$3,303,020						
		Project	\$0							

Scenarios Criteria:

Criteria:

Year	PM	Budget	Rehabilitation	Preventative Maintenance	Surplus PM	Deferred	Stop Gap			
2030	10%	\$4,000,000	II	\$0	Non-Project	\$500,300	\$0	\$11,132,841	Funded	\$0
			III	\$0					Unmet	\$0
			IV	\$0	Project	\$214				
			V	\$3,498,649						
			Total Project	\$3,498,649						
2031	7%	\$3,000,000	II	\$0	Non-Project	\$230,563	\$0	\$10,994,975	Funded	\$0
			III	\$1,662,968					Unmet	\$0
			IV	\$617,863	Project	\$0				
			V	\$488,181						
			Total Project	\$2,769,012						

## Summary

Functional Class	Rehabilitation	Prev. Maint.	Funded Stop Gap	Unmet Stop Gap
Arterial	\$8,232,988	\$422,068	\$0	\$13,675
Collector	\$9,498,483	\$1,027,292	\$0	\$43,765
Other	\$1,473,804	\$72,273	\$0	\$27,976
Residential/Local	\$5,266,900	\$797,592	\$0	\$77,014
<b>Grand Total:</b>	<b>\$24,472,175</b>	<b>\$2,319,225</b>	<b>\$0</b>	<b>\$162,430</b>



# Scenarios - Network Condition Summary

Interest: 3%

Inflation: 3%

Printed: 1/28/2022

Scenario: 2021 S2 IMPROVE PCI TO 75

Year	Budget	PM	Year	Budget	PM	Year	Budget	PM
2022	\$950,000	0%	2026	\$3,400,000	0%	2030	\$4,000,000	10%
2023	\$1,300,000	50%	2027	\$4,700,000	0%	2031	\$3,000,000	7%
2024	\$850,000	10%	2028	\$4,100,000	10%			
2025	\$1,100,000	20%	2029	\$3,399,996	0%			

## Projected Network Average PCI by Year

Year	Never Treated	With Selected Treatment	Treated Centerline Miles	Treated Lane Miles
2022	65	66	1.14	2.34
2023	62	68	6.03	11.91
2024	60	69	3.03	5.82
2025	58	68	3.44	6.98
2026	56	69	6.42	12.37
2027	54	70	5.20	10.39
2028	51	72	8.12	15.94
2029	49	72	4.20	8.08
2030	47	74	6.06	12.10
2031	45	75	8.66	17.16

## Percent Network Area by Functional Class and Condition Category

Condition in base year 2022, prior to applying treatments.

Condition	Arterial	Collector	Res/Loc	Other	Total
I	2.9%	20.5%	30.7%	1.6%	55.7%
II / III	3.8%	8.2%	7.2%	0.0%	19.2%
IV	5.5%	1.0%	4.0%	0.4%	10.7%
V	0.5%	2.4%	5.5%	6.0%	14.3%
Total	12.6%	32.1%	47.4%	8.0%	100.0%

Condition in year 2022 after schedulable treatments applied.

Condition	Arterial	Collector	Res/Loc	Other	Total
I	3.8%	21.2%	31.2%	1.8%	58.0%
II / III	3.8%	8.2%	6.7%	0.0%	18.7%
IV	4.9%	1.0%	4.0%	0.4%	10.2%
V	0.1%	1.7%	5.5%	5.8%	13.1%
Total	12.6%	32.1%	47.4%	8.0%	100.0%

Condition in year 2031 after schedulable treatments applied.

Condition	Arterial	Collector	Res/Loc	Other	Total
I	12.6%	26.7%	37.2%	8.0%	84.5%
II / III	0.0%	2.2%	1.8%	0.0%	4.0%
IV	0.0%	0.0%	0.6%	0.0%	0.6%
V	0.0%	3.2%	7.8%	0.0%	11.0%
Total	12.6%	32.1%	47.4%	8.0%	100.0%

Scenarios Criteria:

Criteria:

## Appendix E

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### PAVEMENT CONDITION MAPS

## **Current Network Condition – 2021**

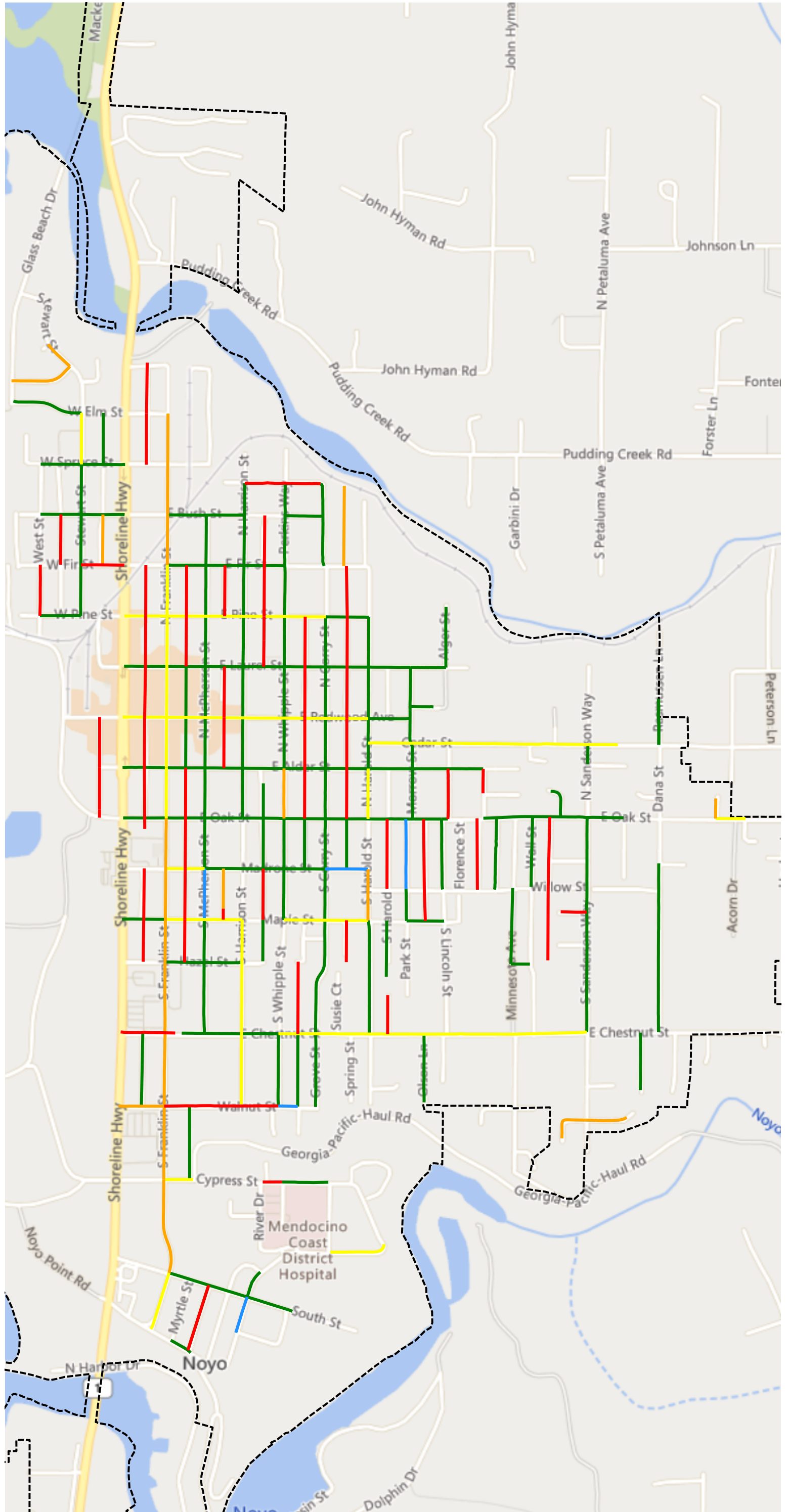


# Current PCI Condition

Printed: 1/28/2022

### Feature Legend

- Category I - Very Good
- Category II - Good (Non-Load)
- Category III - Good (Load)
- Category IV - Poor
- Category V - Very Poor



**Scenario 1: Existing Budget**  
Projected Street Network Condition – 2031

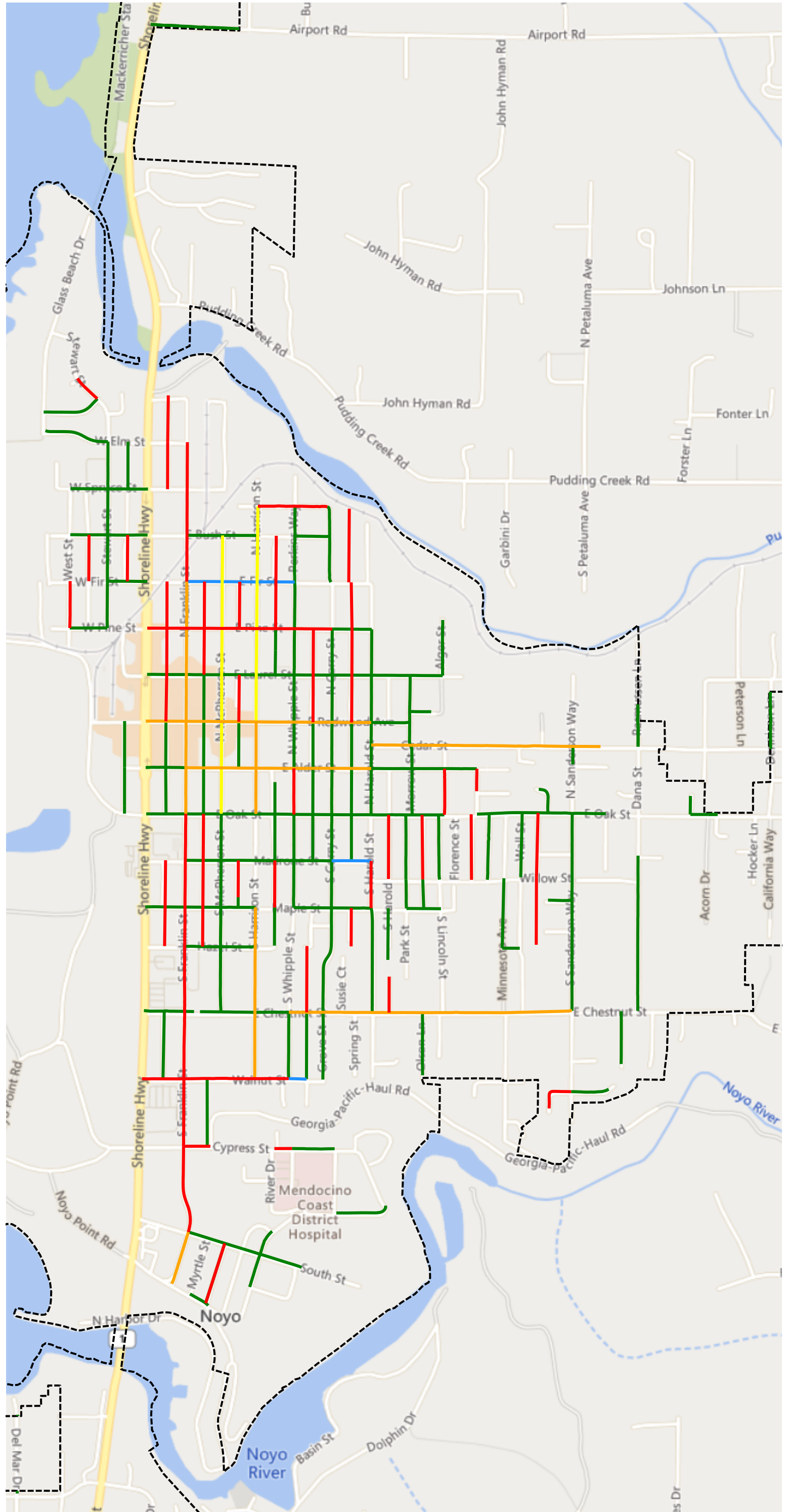


# Scenario PCI Condition

2021 S1: CITY'S EXISTING FUNDING (\$1.0M/YEAR) - 2031 Project Period - Total Rehab for 2031: \$789,814 - Printed: 1/28/2022

### Feature Legend

- Category I - Very Good
- Category II - Good (Non-Load)
- Category III - Good (Load)
- Category IV - Poor
- Category V - Very Poor



**Scenario 2: Improve PCI to 75**  
Projected Street Network Condition – 2031

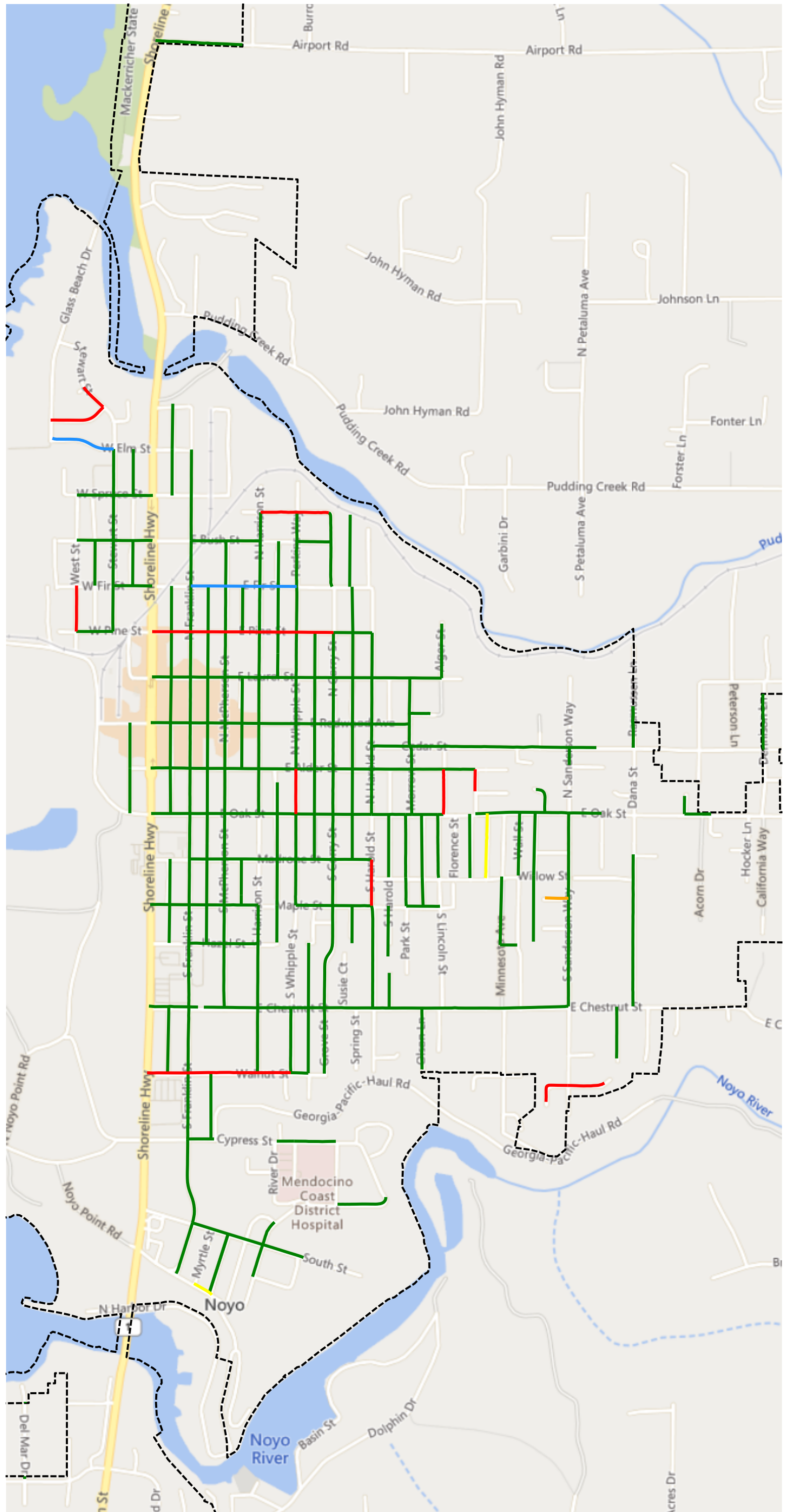


# Scenario PCI Condition

2021 S2 IMPROVE PCI TO 75 - 2031 Project Period - Total Rehab for 2031: \$2,769,012 - Printed: 1/28/2022

### Feature Legend

- Category I - Very Good
- Category II - Good (Non-Load)
- Category III - Good (Load)
- Category IV - Poor
- Category V - Very Poor





## Appendix F

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### SECTIONS SELECTED FOR TREATMENT – SCENARIO 1

# Scenarios - Sections Selected for Treatment

Interest: 3.00%

Inflation: 3.00%

Printed: 1/28/2022

Scenario: 2021 S1: CITY'S EXISTING FUNDING (\$1.0M/YEAR)

Year	Budget	PM	Year	Budget	PM	Year	Budget	PM
2022	\$1,000,000	12%	2026	\$1,000,000	8%	2030	\$1,000,000	5%
2023	\$1,000,000	12%	2027	\$1,000,000	2%	2031	\$1,000,000	5%
2024	\$1,000,000	5%	2028	\$1,000,000	5%			
2025	\$1,000,000	8%	2029	\$1,000,000	2%			

Year: 2022

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment	
											Current PCI	PCI Before	PCI After				
SNUG HARBOR PLACE	OCEAN VIEW DR	SOUTH END	SNUGH	001	110	19	2,090	R	AC		93	93	97	\$1,278	2,755	SLURRY SEAL W/ CRACK SEAL	
											Treatment Total		\$1,278				
PARK ST. 003	END OF 002 0+00	END OF ST. 1+45 PARK		003	145	14	2,030	R	AC	0	66	66	76	\$2,594	8,991	MICROSURFACING W/ CRACK SEAL AND 5% DIGOUTS	
											Treatment Total		\$2,594				
**BOATYARD DR. 002	N.SIDE DRIVE WY HARVEST 0+00	N.F. HWY20 7+85 BOATYA		002	785	41	32,185	A	AC/AC	0	48	48	100	\$287,877	8,750	THICK MILL AND OVERLAY	
											Treatment Total		\$287,877				
**CHESTNUT ST. 001	E.F. CURB HWY1 0+00	END OF CURB-GUTTER S. SIDEWALK	CHESTN	001	498	40	19,920	A	AC	0	5	5	100	\$236,827	6,292	12" FDR W/ 4" HMA	
											Treatment Total		\$236,827				
**WEST FIR ST.	E.F.@ ALLEY 0+00	W.F. HWY1 8+25	WFIR	001	900	43	38,700	C	AC	0	3	3	100	\$370,875	6,119	8" FDR W/ 3.5" HMA	
											Treatment Total		\$370,875				
**AZALEA CIRCLE	W.F. CURB S. SANDERSON 0+00	END OF CULDESAC	AZALEA	001	275	38	10,450	R	AC	0	15	15	100	\$90,277	5,712	4.5" SURFACE RECONSTRUCT	
											Treatment Total		\$90,277				
DELMAR DR	OCEAN VIEW DR	END SOUTH	DELM	002	242	30	7,260	R	AC		88	88	89	\$13	1,319,344	SEAL CRACKS	
NOYO POINT RD	CYPRESS ST	END AT LOT	NOYOPT	001	1,828	24	43,872	R	AC		93	94	94	\$42	992,905	SEAL CRACKS	
											Treatment Total		\$55				
Year 2022 Area Total									156,507		Year 2022 Total			\$989,783			

\*\* - Treatment from Project Selection

# Scenarios - Sections Selected for Treatment

Interest: 3.00%

Inflation: 3.00%

Printed: 1/28/2022

Scenario: 2021 S1: CITY'S EXISTING FUNDING (\$1.0M/YEAR)

Year: 2023

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment	
											Current PCI	PCI Before	PCI After				
MAPLE ST. 005	ALLEY BETWN WHIPPLE HARRISON	S. FRANKLIN	MAPLE	005A	900	49	44,100	C	AC	0	68	66	100	\$251,089	8,571	AC OVERLAY 2 INCHES	
												<b>Treatment Total</b>	<b>\$251,089</b>				
AIRPORT ROAD	W/C.L. SIGN 5+87	E.EDGE HWY1 @ LIMIT LINE 8+55	AIRPOR	001B	268	22	5,896	R	AC	0	93	91	96	\$3,712	5,960	SLURRY SEAL W/ CRACK SEAL	
BOATYARD DR. 001	5+30 FORM HWY1 0+00	4+32 FROM BEGIN	BOATYA	001	432	24	10,368	A	AC/AC	0	92	89	95	\$7,713	13,070	SLURRY SEAL W/ CRACK SEAL	
EAST ALDER 001	E.F CURB HWY 1 0+00	W.F.CURB N.FRANKLIN ST. 3+40	EALDER	001	340	43	14,620	C	AC	0	83	82	89	\$10,040	26,615	SLURRY SEAL W/ CRACK SEAL	
EAST CYPRESS ST. 001	END OF KEMPEE @ VALVE BOX 0+0	W.F.CURB RIVER DRV 12+96	ECYPRES	001	1,296	40	51,840	C	AC/AC	0	81	80	88	\$35,597	31,682	SLURRY SEAL W/ CRACK SEAL	
NORTH HARBOR DRIVE 002	E.F.CURB S FRANKLIN ST. 0+00	E.F.CURB HWY 1 4+55	NHARBO	002	455	32	14,560	R	AC	0	81	80	87	\$9,165	17,685	SLURRY SEAL W/ CRACK SEAL	
NORTH HAROLD ST. 006	LAUREL ST	ALDER ST	NHAROL	006	920	52	47,840	C	AC/AC	0	74	73	82	\$32,851	31,155	SLURRY SEAL W/ CRACK SEAL	
EAST OAK ST. 001	E.F. CURB HWY 1 0+00	W.F.CURB MCKINLEY 32+25	OAK	001	3,225	45	145,125	A	AC	0	81	79	87	\$107,957	22,659	SLURRY SEAL W/ CRACK SEAL	
PERKINS WAY	N.F. CURB E. FIR ST. 0+00	@ DEAD END 10+53	PERKIN	001	1,053	33	34,749	R	AC	0	79	78	86	\$21,873	18,330	SLURRY SEAL W/ CRACK SEAL	
WALL ST. 001	WILLOW	N.F. CURB@ALLEY ST 13+50	WALL	001B	650	38	24,700	R	AC/AC	0	79	78	86	\$15,548	22,850	SLURRY SEAL W/ CRACK SEAL	
WALL ST. 002	E.F. CURB @ ALLEY 0+00	N.F. CURB CHESTNUT ST.	WALL	002	700	38	26,600	R	AC/AC	0	81	80	88	\$16,744	24,088	SLURRY SEAL W/ CRACK SEAL	
												<b>Treatment Total</b>	<b>\$261,200</b>				
HAZELWOOD ST.	S.F. SOUTH ST. 0+00	HYDRANT @ END OF ST.3+60	HAZELW	001	360	28	10,080	R	AC	0	64	63	73	\$9,229	15,147	MICROSURFACING W/ CRACK SEAL AND 3% DIGOUTS	
MADRONE ST. 002	CORRY ST	W.F. CURB @ HAROLD 13+00	MADRON	010	400	44	17,600	R	AC	0	63	61	72	\$16,114	12,083	MICROSURFACING W/ CRACK SEAL AND 3% DIGOUTS	
OCEAN VIEW DRV.WEST 001	W.F. CURB HWY1 0+00	4+18 FROM W.F. HWY1	OCEANV	001	418	36	15,048	R	AC/AC	0	68	67	76	\$13,778	14,815	MICROSURFACING W/ CRACK SEAL AND 3% DIGOUTS	
WALNUT ST. 001	W.F.CURB GROVE 0+00	WHIPPLE	WALNUT	001	340	39	13,260	R	AC	0	62	60	71	\$12,141	11,984	MICROSURFACING W/ CRACK SEAL AND 3% DIGOUTS	
												<b>Treatment Total</b>	<b>\$51,262</b>				
MADRONE ST. 001	E.F.CURB @ HWY 1 0+00	W.F.CURB @ McPHERSON ST.6+50	MADRON	001	650	43	27,950	R	AC	0	67	65	75	\$36,786	8,672	MICROSURFACING W/ CRACK SEAL AND 5% DIGOUTS	

\*\* - Treatment from Project Selection

# Scenarios - Sections Selected for Treatment

Interest: 3.00%

Inflation: 3.00%

Printed: 1/28/2022

Scenario: 2021 S1: CITY'S EXISTING FUNDING (\$1.0M/YEAR)

Year: 2023

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment	
											Current PCI	PCI Before	PCI After				
MANZANITA ST.	W.F. CURB N. FRANKLIN ST.0+00	E.F. CURB HWY1 3+30	MANZAN	001	330	26	8,580	R	AC	0	69	67	77	\$11,293	8,800	MICROSURFACING W/ CRACK SEAL AND 5% DIGOUTS	
WILLOW ST. 001	W.F.CURB S.SANDERSON 0+00	E.EDGE ALLEY LIVINGSTON 6+23	WILLOW	001	623	32	19,936	R	AC	0	67	65	75	\$26,238	8,672	MICROSURFACING W/ CRACK SEAL AND 5% DIGOUTS	
<b>Treatment Total</b>													<b>\$74,317</b>				
CEDAR ST. 002	END CURB 1320 CEDAR 0+00	1631 EAST PROP. LINE 8+90	CEDAR	002	350	19	6,650	R	AC	0	42	40	100	\$67,734	5,007	THICK MILL AND OVERLAY	
HARBOR AVE	OCEAN VIEW DR	SOUTH END	HARAV	001	90	39	3,510	R	AC/AC	0	49	47	100	\$35,752	4,778	THICK MILL AND OVERLAY	
LONNE WAY	E.F. SNADERSON 0+00	END OF CULDESAC 4+87	LONNE	001	487	32	15,584	R	AC	0	42	40	100	\$158,732	5,009	THICK MILL AND OVERLAY	
N100D	E OAK ST	E ALDER ST	N100D	N100D1	417	20	8,340	O	AC		21	18	100	\$35,077	12,490	THICK MILL AND OVERLAY	
N102C	N101C	MAIN ST	N102C	N102C1	146	26	3,796	O	AC		16	13	100	\$15,966	12,490	THICK MILL AND OVERLAY	
<b>Treatment Total</b>													<b>\$313,261</b>				
HAZEL ST.	S HARRISON ST	EST END	HAZEL	004	155	12	1,860	R	AC	0	5	1	100	\$24,321	3,774	4.5" SURFACE RECONSTRUCT	
<b>Treatment Total</b>													<b>\$24,321</b>				
N300E	E REDWOOD AVE	E LAUREL ST	N300E	N300E1	416	18	7,488	O	AC		94	91	96	\$2,571	9,972	SINGLE CHIP SEAL	
S100J	MADRONE ST	E OAK ST	S100J	S100J1	418	14	5,852	O	AC		94	91	96	\$2,010	9,954	SINGLE CHIP SEAL	
S301N	S302N	WILLOW ST	S301N	S301N1	652	12	7,824	O	AC		94	91	96	\$2,687	10,523	SINGLE CHIP SEAL	
S302N	S301N	WALL ST	S302N	S302N1	139	16	2,224	O	AC		94	91	96	\$764	9,917	SINGLE CHIP SEAL	
<b>Treatment Total</b>													<b>\$8,032</b>				
<b>Year 2023 Area Total</b>										<b>595,980</b>	<b>Year 2023 Total</b>		<b>\$983,482</b>				

Year: 2024

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment	
											Current PCI	PCI Before	PCI After				
SOUTH LINCOLN ST.	MAPLE ST	WILLOW ST	SLINCO	015A	282	30	8,460	C	AC	0	68	63	100	\$49,613	8,665	AC OVERLAY 2 INCHES	
<b>Treatment Total</b>													<b>\$49,613</b>				
CHESTNUT ST. 002	END CURB-GUTTER@001, 0+00	S. WHIPPLE	CHESTN	005	884	32	28,288	C	AC/AC	0	83	80	87	\$20,008	22,454	SLURRY SEAL W/ CRACK SEAL	

\*\* - Treatment from Project Selection

# Scenarios - Sections Selected for Treatment

Interest: 3.00%

Inflation: 3.00%

Printed: 1/28/2022

Scenario: 2021 S1: CITY'S EXISTING FUNDING (\$1.0M/YEAR)

Year: 2024

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment
											Current PCI	PCI Before	PCI After			
DUBOIS LANE	S.F. CURB WALNUT 0+00	END OF PAVEMENT 2+07	DUBOIS	001	207	25	5,175	R	AC/AC	0	94	90	95	\$3,356	10,253	SLURRY SEAL W/ CRACK SEAL
EAST LAUREL 002	E.F.CURB N.HARRISON 0+00	W.F.CURB N.HAROLD 10+05	ELAURE	002	1,005	40	40,200	C	AC/AC	0	75	72	81	\$28,433	24,630	SLURRY SEAL W/ CRACK SEAL
EAST OAK ST. 002	E.F. CURB McKINLEY ST. 0+00	BACK E.SIDE SDWLK. DANA 17+25	OAK	002	1,725	40	69,000	R	AC	0	77	74	82	\$44,735	18,270	SLURRY SEAL W/ CRACK SEAL
<b>Treatment Total</b>													<b>\$96,532</b>			
CHESTNUT ST. 003	SANDERSON WY	END OF STREET (9+75)	CHESTN	015	975	32	31,200	C	AC/AC	0	73	70	79	\$50,570	10,029	RUBBERIZED CHIP SEAL+MICROSURFACING W/ 3% DIGOUTS
WEST ELM ST.	W.F. CURB HWY1 0+00	W.EDGE PAVEMENT 10+30	WELM	001	1,030	43	44,290	C	AC	0	73	68	78	\$71,787	6,987	RUBBERIZED CHIP SEAL+MICROSURFACING W/ 3% DIGOUTS
<b>Treatment Total</b>													<b>\$122,357</b>			
JEWETT ST.	S.F.SIDEWALK OAK ST.0+00	END OF CULDESAC 5+36	JEWETT	001	536	39	20,904	R	AC	0	72	69	78	\$28,338	8,626	MICROSURFACING W/ CRACK SEAL AND 5% DIGOUTS
LIVINGSTON ST.	S.F.CURB OAK 0+00	N.F.CURB WILLOW ST.6+17	LIVING	001	617	35	21,595	R	AC/AC	0	72	69	78	\$29,275	8,747	MICROSURFACING W/ CRACK SEAL AND 5% DIGOUTS
NORTH HARBOR DRIVE 001	CITY LIMITS @GUARD RAIL 0+00	E.F.CURB S. FRANKLIN 19+90	NHARBO	001	1,990	28	55,720	R	AC	0	72	69	78	\$75,534	8,626	MICROSURFACING W/ CRACK SEAL AND 5% DIGOUTS
<b>Treatment Total</b>													<b>\$133,147</b>			
DENNISON LANE	S.EDGE OF PAVEMANT 0+00	N.W.PROP LN. STANLEY 8+53	DENNIS	001	853	14	11,942	R	AC	0	46	41	100	\$125,285	4,839	THICK MILL AND OVERLAY
JOHN CIMOLINO WAY	STEWART ST	END	JOHNCIMO	001	286	36	10,296	R	AC		49	44	100	\$108,017	4,769	THICK MILL AND OVERLAY
N100E	E OAK ST	E ALDER ST	N100E	N100E1	413	16	6,608	O	AC		6	0	100	\$28,626	12,126	THICK MILL AND OVERLAY
S202F	MAPLE ST	105' N/O MAPLE ST	S202F	S202F1	105	14	1,470	O	AC		1	0	100	\$6,369	12,126	THICK MILL AND OVERLAY
WALNUT ST.	GROVE ST	EAST END	WALNUT	004	188	43	8,084	R	AC/AC	0	51	46	100	\$84,811	4,710	THICK MILL AND OVERLAY
WEST CYPRESS	WEST END	S MAIN ST	WCYPST	001	165	50	8,250	C	AC	0	51	43	100	\$109,406	4,431	THICK MILL AND OVERLAY
<b>Treatment Total</b>													<b>\$462,514</b>			
CHIEF CELERI DR	W OAK ST	W ALDER ST	CELERI	001	475	15	7,125	A	AC		4	0	100	\$121,573	4,384	12" FDR W/ 4" HMA
<b>Treatment Total</b>													<b>\$121,573</b>			

\*\* - Treatment from Project Selection

# Scenarios - Sections Selected for Treatment

Interest: 3.00%

Inflation: 3.00%

Printed: 1/28/2022

Scenario: 2021 S1: CITY'S EXISTING FUNDING (\$1.0M/YEAR)

## Year: 2024

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Current PCI	Treatment		Cost	Rating	Treatment
												PCI Before	PCI After			
S500D	WALNUT ST	E CHESTNUT ST	S500D	S500D1	614	16	9,824	O	AC		94	89	95	\$3,475	15,123	SINGLE CHIP SEAL
													<b>Treatment Total</b>	<b>\$3,475</b>		
MAPLE ST. 001	W.F.CURB S.LINCOLN 0+00	S.HAROLD	MAPLE	001A	640	45	28,800	C	AC/AC	0	95	88	89	\$5	30,729,948	SEAL CRACKS
													<b>Treatment Total</b>	<b>\$5</b>		
<b>Year 2024 Area Total</b>									<b>417,231</b>	<b>Year 2024 Total</b>			<b>\$989,216</b>			

## Year: 2025

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Current PCI	Treatment		Cost	Rating	Treatment
												PCI Before	PCI After			
EAST ELM ST.	E.F.@ HWY1 0+00	W.F. CURB N FRANKLIN ST 3+25	EELM	001	325	44	14,300	C	AC	0	75	68	100	\$86,378	7,792	AC OVERLAY 2 INCHES
MAPLE ST. 001	S.HAROLD	ALLEY BETWN WHIPPLE HARRISON	MAPLE	001B	1,000	45	45,000	C	AC	0	69	61	100	\$271,816	8,610	AC OVERLAY 2 INCHES
													<b>Treatment Total</b>	<b>\$358,194</b>		
DELMAR DR	OCEAN VIEW DR	END SOUTH	DELM	002	242	30	7,260	R	AC		88	86	92	\$4,849	17,716	SLURRY SEAL W/ CRACK SEAL
EAST ALDER 002	HAROLD	@HYDRANT END OF ST. 28+45	EALDER	002B	1,261	43	54,223	R	AC	0	81	76	85	\$36,209	17,498	SLURRY SEAL W/ CRACK SEAL
EAST LAUREL ST. 001	E.F. CURB HWY1 0+00	W.F. CURB HARRISON ST 9+25	ELAURE	001	925	45	41,625	C	AC	0	87	81	88	\$30,324	18,232	SLURRY SEAL W/ CRACK SEAL
MAPLE ST. 005	S. FRANKLIN	E.F. CURB HWY 1	MAPLE	005B	400	49	19,600	C	AC	0	81	74	82	\$14,279	17,032	SLURRY SEAL W/ CRACK SEAL
SOUTH McPHERSON ST.	MAPLE	N.F. CURB CHESTNUT ST.	SMcPHE	003	1,042	44	45,848	C	AC	0	90	84	91	\$33,400	17,821	SLURRY SEAL W/ CRACK SEAL
WEST PINE ST.	END SIDE WALK N.SIDE 0+00	W.F. CURB HWY 1 2+58	WPINE	002	258	43	11,094	C	AC	0	80	73	81	\$8,082	16,709	SLURRY SEAL W/ CRACK SEAL
													<b>Treatment Total</b>	<b>\$127,143</b>		
EAST SPRUCE ST.	E.F. CURB HWY1 0+00	CEMETARY GATE 5+07	ESPRUC	001	507	45	22,815	R	AC	0	49	42	100	\$246,536	4,690	THICK MILL AND OVERLAY
HAZEL ST.	E.F. CURB HWY1 0+00	W.F. CURB S.FRANKLIN 3+40	HAZEL	001	340	32	10,880	R	AC	0	51	44	100	\$117,568	4,636	THICK MILL AND OVERLAY
N100H	E OAK ST	E ALDER ST	N100H	N100H1	414	16	6,624	O	AC		2	0	100	\$29,557	11,773	THICK MILL AND OVERLAY

\*\* - Treatment from Project Selection

# Scenarios - Sections Selected for Treatment

Interest: 3.00%

Inflation: 3.00%

Printed: 1/28/2022

Scenario: 2021 S1: CITY'S EXISTING FUNDING (\$1.0M/YEAR)

Year: 2025

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment	
											Current PCI	PCI Before	PCI After				
N200F	E ALDER ST	E REDWOOD AVE	N200F	N200F1	407	14	5,698	O	AC		4	0	100	\$25,425	11,773	THICK MILL AND OVERLAY	
													Treatment Total		\$419,086		
DICKWILLIAMS WAY	DANA ST	END AT CDS	DICKWI	001	185	28	5,180	R	AC		9	0	100	\$71,855	3,557	4.5" SURFACE RECONSTRUCT	
													Treatment Total		\$71,855		
S500H	WALNUT ST	E CHESTNUT ST	S500H	S500H1	635	18	11,430	O	AC		94	87	93	\$4,164	19,067	SINGLE CHIP SEAL	
													Treatment Total		\$4,164		
**BOATYARD DR. 002	N.SIDE DRIVE WY HARVEST 0+00	N.F. HWY20 7+85	BOATYA	002	785	41	32,185	A	AC/AC	0	48	87	88	\$33	2,550,889	SEAL CRACKS	
**CHESTNUT ST. 001	E.F. CURB HWY1 0+00	END OF CURB-GUTTER S. SIDEWALK	CHESTN	001	498	40	19,920	A	AC	0	5	87	88	\$43	1,026,195	SEAL CRACKS	
S100H	MADRONE ST	E OAK ST	S100H	S100H1	410	14	5,740	O	AC		96	88	89	\$18	1,187,114	SEAL CRACKS	
S300G	HAZEL ST	MAPLE ST	S300G	S300G1	373	16	5,968	O	AC		96	88	89	\$18	1,187,114	SEAL CRACKS	
S300K	S END	MAPLE ST	S300K	S300K1	579	13	7,527	O	AC		96	88	89	\$23	1,187,114	SEAL CRACKS	
**WEST FIR ST.	E.F.@ ALLEY 0+00	W.F. HWY1 8+25	WFIR	001	900	43	38,700	C	AC	0	3	91	92	\$53	811,288	SEAL CRACKS	
													Treatment Total		\$188		
<b>Year 2025 Area Total</b>									<b>411,617</b>		<b>Year 2025 Total</b>			<b>\$980,630</b>			

Year: 2026

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment	
											Current PCI	PCI Before	PCI After				
EAST PINE ST.	CORRY ST	W.F. CURB HAROLD ST 19+30	EPINE	010	356	43	15,308	C	AC	0	79	69	100	\$95,240	7,442	AC OVERLAY 2 INCHES	
													Treatment Total		\$95,240		
EDDY ST.	W.F. CURB S SANDERSON 0+00	END OF STREET 3+18	EDDY	001	318	41	13,038	R	AC	0	82	76	84	\$8,968	17,070	SLURRY SEAL W/ CRACK SEAL	
EAST FIR ST. 002	RR TRACKS	W.F.CURB H FRANKLIN	EFIR	003	192	44	8,448	C	AC/AC	0	92	84	91	\$6,339	17,645	SLURRY SEAL W/ CRACK SEAL	
NORTH CORRY ST.	N.F.CURB OAK ST.0+00	S.F.CURB FIR ST. 22+47	NCORRY	001	2,247	42	94,374	R	AC	0	79	73	81	\$64,912	17,219	SLURRY SEAL W/ CRACK SEAL	
OCEAN VIEW DRV.EAST 003	E.F. CURB HWY 1 0+00	5+30 FROM HWY1	OCEANV	003	530	26	13,780	R	AC	0	82	76	84	\$9,479	17,069	SLURRY SEAL W/ CRACK SEAL	

\*\* - Treatment from Project Selection

# Scenarios - Sections Selected for Treatment

Interest: 3.00%

Inflation: 3.00%

Printed: 1/28/2022

Scenario: 2021 S1: CITY'S EXISTING FUNDING (\$1.0M/YEAR)

Year: 2026

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment
											Current PCI	PCI Before	PCI After			
PARK ST. 002	S.F. CURB WILLOW 0+00	WHERE ST.NARROWS 300 BLK 5+68	PARK	002	568	28	15,904	R	AC	0	81	75	83	\$10,939	17,177	SLURRY SEAL W/ CRACK SEAL
SOUTH DANA ST.	N.F. CUEB CHESTNUT ST 0+00	S.F. CURB OAK ST. 19+00	SDANA	001	1,900	40	76,000	C	AC	0	91	83	90	\$57,026	17,524	SLURRY SEAL W/ CRACK SEAL
SOUTH SANDERSON ST. 001	N.F. CURB CHESTNUT ST 0+00	S.F. CURB OAK ST 19+15	SSANDE	001	1,915	40	76,600	C	AC	0	91	83	90	\$57,476	17,525	SLURRY SEAL W/ CRACK SEAL
SOUTH WHIPPLE ST. 002	N.SIDE HAZEL RT OF WAY 0+00	N.F. CURB WALNUT ST.12+85	SWHIP	002	1,285	32	41,120	R	AC	0	82	76	84	\$28,283	17,070	SLURRY SEAL W/ CRACK SEAL
<b>Treatment Total</b>												<b>\$243,422</b>				
EAST FIR ST. 001	E.F.CURB @ N FRANKLIN ST	E.F.CURB HAROLD 17+45	EFIR	001	1,745	43	75,035	C	AC	0	77	66	76	\$129,025	6,315	RUBBERIZED CHIP SEAL+MICROSURFACING W/ 3% DIGOUTS
<b>Treatment Total</b>												<b>\$129,025</b>				
N100J	E OAK ST	E ALDER ST	N100J	N100J1	418	16	6,688	O	AC		0	0	100	\$30,737	11,430	THICK MILL AND OVERLAY
N401G	214' N/O LAUREL ST	E PINE ST	N401G	N401G1	206	14	2,884	O	AC		20	7	100	\$13,255	11,430	THICK MILL AND OVERLAY
NANCY WAY	N.F.CURB CEDAR 0+00	END OF PAVEMENT @ OSPREY 2+66	NANCY	001	266	31	8,246	R	AC	0	54	45	100	\$91,779	4,473	THICK MILL AND OVERLAY
STEWART ST. 002	S.F. CURB W. ELM 0+00	SPRUCE	STEWAR	002	413	43	17,759	R	AC/AC	0	53	44	100	\$197,659	4,501	THICK MILL AND OVERLAY
<b>Treatment Total</b>												<b>\$333,430</b>				
EAST FIR ST. 002	E.F.CURB @ HWY 1 0+00	RR TRACKS	EFIR	002	142	44	6,248	C	AC	0	33	11	100	\$92,200	3,974	8" FDR W/ 3.5" HMA
<b>Treatment Total</b>												<b>\$92,200</b>				
HARBOR AVE	OCEAN VIEW DR	NORTH END	HARAV	002	140	32	4,480	R	AC/AC	0	32	22	100	\$64,009	3,454	4.5" SURFACE RECONSTRUCT
<b>Treatment Total</b>												<b>\$64,009</b>				
N800C	W SPRUCE ST	W ELM ST	N800C	N800C1	422	16	6,752	O	AC		94	86	92	\$2,534	21,914	SINGLE CHIP SEAL
<b>Treatment Total</b>												<b>\$2,534</b>				
**AZALEA CIRCLE	W.F. CURB S. SANDERSON 0+00	END OF CULDESAC	AZALEA	001	275	38	10,450	R	AC	0	15	86	87	\$24	675,842	SEAL CRACKS
BOATYARD DR. 001	5+30 FORM HWY1 0+00	4+32 FROM BEGIN	BOATYA	001	432	24	10,368	A	AC/AC	0	92	87	88	\$8	6,838,530	SEAL CRACKS

\*\* - Treatment from Project Selection



# Scenarios - Sections Selected for Treatment

Interest: 3.00%

Inflation: 3.00%

Printed: 1/28/2022

Scenario: 2021 S1: CITY'S EXISTING FUNDING (\$1.0M/YEAR)

Year: 2026

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment	
											Current PCI	PCI Before	PCI After				
EAST ALDER 001	E.F. CURB HWY 1 0+00	W.F.CURB N.FRANKLIN ST. 3+40	EALDER	001	340	43	14,620	C	AC	0	83	85	86	\$37	1,240,372	SEAL CRACKS	
EAST CYPRESS ST. 001	END OF KEMPEE @ VALVE BOX 0+0	W.F.CURB RIVER DRV 12+96	ECYPRES	001	1,296	40	51,840	C	AC/AC	0	81	85	86	\$96	2,053,165	SEAL CRACKS	
MAPLE ST. 005	ALLEY BETWN WHIPPLE HARRISON	S. FRANKLIN	MAPLE	005A	900	49	44,100	C	AC	0	68	87	88	\$47	1,723,645	SEAL CRACKS	
N102G	300 N/O E OAK ST	E ALDER ST	N102G	N102G1	117	16	1,872	O	AC/AC		96	87	88	\$3	2,809,315	SEAL CRACKS	
NORTH HAROLD ST. 006	LAUREL ST	ALDER ST	NHAROL	006	920	52	47,840	C	AC/AC	0	74	79	81	\$185	1,669,355	SEAL CRACKS	
EAST OAK ST. 001	E.F. CURB HWY 1 0+00	W.F.CURB MCKINLEY 32+25	OAK	001	3,225	45	145,125	A	AC	0	81	80	82	\$515	1,072,512	SEAL CRACKS	
**PENITENTI WAY	W.F. CURB S.SANDERSON 0+00	END OF CULDESAC 2+75	PENITE	001	275	38	10,450	R	AC	0	16	1	2	\$355	0	SEAL CRACKS	
S600E	E END	WALNUT ST	S600E	S600E1	610	12	7,320	O	AC/AC		96	87	88	\$10	2,809,315	SEAL CRACKS	
**SOUTH HAROLD ST. 003	S.F. CHESTNUT ST 0+00	END OF ST.@W.POWER POLE 5+00	SHAROL	003	620	16	9,920	R	AC	0	12	0	0	\$363	0	SEAL CRACKS	
SNUG HARBOR PLACE	OCEAN VIEW DR	SOUTH END	SNUGH	001	110	19	2,090	R	AC		93	86	87	\$5	677,355	SEAL CRACKS	
<b>Treatment Total</b>												<b>\$1,648</b>					
<b>Year 2026 Area Total</b>									<b>838,659</b>	<b>Year 2026 Total</b>			<b>\$961,508</b>				

Year: 2027

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment
											Current PCI	PCI Before	PCI After			
NORTH HAROLD ST. 005	S.F. CURB FIR ST 0+00	LAUREL ST	NHAROL	005	920	52	47,840	C	AC	0	80	67	100	\$306,569	7,417	AC OVERLAY 2 INCHES
<b>Treatment Total</b>												<b>\$306,569</b>				
**BOATYARD DR. 002	N.SIDE DRIVE WY HARVEST 0+00	N.F. HWY20 7+85	BOATYA	002	785	41	32,185	A	AC/AC	0	48	84	91	\$26,948	19,438	SLURRY SEAL W/ CRACK SEAL
**CHESTNUT ST. 001	E.F. CURB HWY1 0+00	END OF CURB-GUTTER S. SIDEWALK	CHESTN	001	498	40	19,920	A	AC	0	5	83	90	\$16,679	17,585	SLURRY SEAL W/ CRACK SEAL
ESPEY WAY	S.F. CURB WALNUT ST 0+00	END OF PAVEMENT 2+58	ESPEY	001	258	36	9,288	R	AC/AC	0	94	85	92	\$6,581	14,847	SLURRY SEAL W/ CRACK SEAL
FLORENCE ST.	S.F. CURB OAK 0+00	N.F. CURB WILLOW ST. 6+03	FLOREN	001	603	31	18,693	R	AC	0	82	74	83	\$13,243	16,693	SLURRY SEAL W/ CRACK SEAL
MAPLE ST. 001	W.F.CURB S.LINCOLN 0+00	S.HAROLD	MAPLE	001A	640	45	28,800	C	AC/AC	0	95	85	92	\$22,259	17,442	SLURRY SEAL W/ CRACK SEAL

\*\* - Treatment from Project Selection

# Scenarios - Sections Selected for Treatment

Interest: 3.00%

Inflation: 3.00%

Printed: 1/28/2022

Scenario: 2021 S1: CITY'S EXISTING FUNDING (\$1.0M/YEAR)

Year: 2027

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment	
											Current PCI	PCI Before	PCI After				
OAK TERRACE COURT	N.F. CURB OAK ST. 0+00	END OF ST. 3+38	OAKTER	001	338	36	12,168	R	AC	0	85	77	85	\$8,621	16,364	SLURRY SEAL W/ CRACK SEAL	
PUDDING CREEK RD.	C.L SIGN0+00	E.F. HWY1 @ LIMIT LINE 9+55	PUDDIN	001	955	23	21,965	R	AC	0	89	81	88	\$15,562	16,682	SLURRY SEAL W/ CRACK SEAL	
SOUTH McPHERSON ST.	S.F. CURB OAK ST.	MADRONE	SMcPHE	001	436	44	19,184	C	AC	0	94	84	91	\$14,827	16,807	SLURRY SEAL W/ CRACK SEAL	
SOUTH SANDERSON ST. 002	412 FT N/O END OF W. SIDEWALK	S.F. CURB CHESTNUT ST 1040	SSANDE	010	628	30	18,840	R	AC	0	84	76	84	\$13,348	16,512	SLURRY SEAL W/ CRACK SEAL	
**WEST FIR ST.	E.F.@ ALLEY 0+00	W.F. HWY1 8+25	WFIR	001	900	43	38,700	C	AC	0	3	88	94	\$29,910	14,999	SLURRY SEAL W/ CRACK SEAL	
<b>Treatment Total</b>													<b>\$167,978</b>				
KEMPEE WAY	433 FT E/O HOSPITAL WY	N. OF VALVE BOX 9+35	KEMPEE	010	502	37	18,574	R	AC	0	59	48	100	\$212,932	4,242	THICK MILL AND OVERLAY	
N402G	E LAUREL ST	214' N/O LAUREL ST	N402G	N402G1	214	14	2,996	O	AC		0	0	100	\$14,183	11,097	THICK MILL AND OVERLAY	
PARK ST. 001	S.F. CURB OAK 0+00	N.F.CURB WILLOW 6+00	PARK	001	600	25	15,000	R	AC/AC	0	55	44	100	\$171,959	4,362	THICK MILL AND OVERLAY	
<b>Treatment Total</b>													<b>\$399,074</b>				
AIRPORT ROAD	W/C.L. SIGN 5+87	E.EDGE HWY1 @ LIMIT LINE 8+55	AIRPOR	001B	268	22	5,896	R	AC	0	93	86	87	\$14	665,892	SEAL CRACKS	
CEDAR ST. 002	END CURB CEDAR 0+00	1631 EAST PROP. LINE 8+90	CEDAR	002	350	19	6,650	R	AC	0	42	87	88	\$6	1,783,624	SEAL CRACKS	
CHIEF CELERI DR	W OAK ST	W ALDER ST	CELERI	001	475	15	7,125	A	AC		4	87	88	\$17	967,288	SEAL CRACKS	
CHESTNUT ST. 002	END CURB-GUTTER@001, 0+00	S. WHIPPLE	CHESTN	005	884	32	28,288	C	AC/AC	0	83	83	84	\$74	1,681,185	SEAL CRACKS	
CHESTNUT ST. 003	SANDERSON WY	END OF STREET (9+75)	CHESTN	015	975	32	31,200	C	AC/AC	0	73	74	77	\$152	924,774	SEAL CRACKS	
EAST LAUREL 002	E.F.CURB N.HARRISON 0+00	W.F.CURB N.HAROLD 10+05	ELAURE	002	1,005	40	40,200	C	AC/AC	0	75	77	79	\$178	1,018,209	SEAL CRACKS	
HARBOR AVE	OCEAN VIEW DR	SOUTH END	HARAV	001	90	39	3,510	R	AC/AC	0	49	87	88	\$3	1,783,624	SEAL CRACKS	
HAZEL ST.	S HARRISON ST	EST END	HAZEL	004	155	12	1,860	R	AC	0	5	86	87	\$5	656,157	SEAL CRACKS	
LONNE WAY	E.F. SNADERSON 0+00	END OF CULDESAC 4+87	LONNE	001	487	32	15,584	R	AC	0	42	87	88	\$13	1,783,624	SEAL CRACKS	
MANZANITA ST.	W.F. CURB N. FRANKLIN ST.0+00	E.F. CURB HWY1 3+30	MANZAN	001	330	26	8,580	R	AC	0	69	70	73	\$49	600,962	SEAL CRACKS	
N100D	E OAK ST	E ALDER ST	N100D	N100D1	417	20	8,340	O	AC		21	87	88	\$11	1,114,765	SEAL CRACKS	
N102C	N101C	MAIN ST	N102C	N102C1	146	26	3,796	O	AC		16	87	88	\$5	1,114,765	SEAL CRACKS	
N300E	E REDWOOD AVE	E LAUREL ST	N300E	N300E1	416	18	7,488	O	AC		94	86	87	\$28	414,967	SEAL CRACKS	

\*\* - Treatment from Project Selection

# Scenarios - Sections Selected for Treatment

Interest: 3.00%

Inflation: 3.00%

Printed: 1/28/2022

Scenario: 2021 S1: CITY'S EXISTING FUNDING (\$1.0M/YEAR)

## Year: 2027

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment		
											Current PCI	PCI Before	PCI After					
NORTH HARBOR DRIVE 002	E.F.CURB S FRANKLIN ST. 0+00	E.F.CURB HWY 1 4+55	NHARBO	002	455	32	14,560	R	AC	0	81	81	83	\$51	712,991	SEAL CRACKS		
OCEAN VIEW DRV.WEST 001	W.F. CURB HWY1 0+00	4+18 FROM W.F. HWY1	OCEANV	001	418	36	15,048	R	AC/AC	0	68	71	74	\$79	911,470	SEAL CRACKS		
PERKINS WAY	N.F. CURB E. FIR ST. 0+00	@ DEAD END10+53	PERKIN	001	1,053	33	34,749	R	AC	0	79	79	81	\$134	705,425	SEAL CRACKS		
S100J	MADRONE ST	E OAK ST	S100J	S100J1	418	14	5,852	O	AC		94	86	87	\$22	414,945	SEAL CRACKS		
S301N	S302N	WILLOW ST	S301N	S301N1	652	12	7,824	O	AC		94	86	87	\$30	415,650	SEAL CRACKS		
S302N	S301N	WALL ST	S302N	S302N1	139	16	2,224	O	AC		94	86	87	\$9	414,904	SEAL CRACKS		
SOUTH LINCOLN ST.	MAPLE ST	WILLOW ST	SLINCO	015A	282	30	8,460	C	AC	0	68	87	88	\$10	1,673,442	SEAL CRACKS		
WALL ST. 001	WILLOW	N.F. CURB@ALLEY ST 13+50	WALL	001B	650	38	24,700	R	AC/AC	0	79	82	83	\$78	960,881	SEAL CRACKS		
WALL ST. 002	E.F. CURB @ ALLEY 0+00	N.F. CURB CHESTNUT ST.	WALL	002	700	38	26,600	R	AC/AC	0	81	84	85	\$62	1,218,672	SEAL CRACKS		
WEST CYPRESS	WEST END	S MAIN ST	WCYPST	001	165	50	8,250	C	AC	0	51	87	88	\$10	1,673,442	SEAL CRACKS		
WEST ELM ST.	W.F. CURB HWY1 0+00	W.EDGE PAVEMENT 10+30	WELM	001	1,030	43	44,290	C	AC	0	73	70	73	\$250	560,213	SEAL CRACKS		
												Treatment Total		\$1,290				
Year 2027 Area Total									665,227		Year 2027 Total			\$874,911				

## Year: 2028

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment		
											Current PCI	PCI Before	PCI After					
SOUTH ST. 005	E. EDGE VALLEY GUTTER HWY 1	HAZELWOOD ST	SOUTH	005	1,218	38	46,284	C	AC	0	80	64	100	\$305,496	7,521	AC OVERLAY 2 INCHES		
												Treatment Total		\$305,496				
BOATYARD DR. 001	5+30 FORM HWY1 0+00	4+32 FROM BEGIN	BOATYA	001	432	24	10,368	A	AC/AC	0	92	86	92	\$8,942	19,232	SLURRY SEAL W/ CRACK SEAL		
EAST ALDER 001	E.F.CURB HWY 1 0+00	W.F.CURB N.FRANKLIN ST. 3+40	EALDER	001	340	43	14,620	C	AC	0	83	84	91	\$11,639	23,527	SLURRY SEAL W/ CRACK SEAL		
EAST CYPRESS ST. 001	END OF KEMPEE @ VALVE BOX 0+0	W.F.CURB RIVER DRV 12+96	ECYPRES	001	1,296	40	51,840	C	AC/AC	0	81	84	91	\$41,267	25,954	SLURRY SEAL W/ CRACK SEAL		
NORTH HAROLD ST. 006	LAUREL ST	ALDER ST	NHAROL	006	920	52	47,840	C	AC/AC	0	74	79	87	\$38,083	28,934	SLURRY SEAL W/ CRACK SEAL		
OCEAN VIEW DRV.WEST 001	W.F. CURB HWY1 0+00	4+18 FROM W.F. HWY1	OCEANV	001	418	36	15,048	R	AC/AC	0	68	73	82	\$10,981	19,010	SLURRY SEAL W/ CRACK SEAL		

\*\* - Treatment from Project Selection

# Scenarios - Sections Selected for Treatment

Interest: 3.00%

Inflation: 3.00%

Printed: 1/28/2022

Scenario: 2021 S1: CITY'S EXISTING FUNDING (\$1.0M/YEAR)

Year: 2028

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment
											Current PCI	PCI Before	PCI After			
RASSMUSSEN LANE	N EDGE CEDAR 0+00	END OF GOOD PAVEMENT 4+55	RASSMU	001	455	13	5,915	R	AC/AC	0	94	83	90	\$4,317	15,571	SLURRY SEAL W/ CRACK SEAL
WALL ST. 002	E.F. CURB @ ALLEY 0+00	N.F. CURB CHESTNUT ST.	WALL	002	700	38	26,600	R	AC/AC	0	81	84	91	\$19,410	18,882	SLURRY SEAL W/ CRACK SEAL
<b>Treatment Total</b>													<b>\$134,639</b>			
FRONTAGE ROAD(UN-NAMED)	N.END @ LIMIT LINE 0+00	N.F. CURB OCEAN VIEW. W 6+80	FRONTA	001	680	24	16,320	R	AC	0	59	48	100	\$192,705	4,128	THICK MILL AND OVERLAY
N101C	W OAK ST	W ALDER ST	N101C	N101C1	420	16	6,720	O	AC		17	0	100	\$32,765	10,774	THICK MILL AND OVERLAY
N200C	W ALDER ST	W REDWOOD AVE	N200C	N200C1	439	17	7,463	O	AC		9	0	100	\$36,388	10,774	THICK MILL AND OVERLAY
NORTH DANA ST	EAST OAK ST	END	NDANA	001	319	28	8,932	R	AC		60	47	100	\$105,468	4,152	THICK MILL AND OVERLAY
S200L	MAPLE ST	WILLOW ST	S200L	S200L1	242	14	3,388	O	AC		0	0	100	\$16,519	10,774	THICK MILL AND OVERLAY
<b>Treatment Total</b>													<b>\$383,845</b>			
HOWLAND CT.	N.F. CURB OAK ST.0+00	END OF CULDESAC 2+30	HOWLAN	001	230	36	8,280	R	AC	0	28	8	100	\$125,507	3,255	4.5" SURFACE RECONSTRUCT
<b>Treatment Total</b>													<b>\$125,507</b>			
DENNISON LANE	S.EDGE OF PAVEMANT 0+00	N.W.PROP LN. STANLEY 8+53	DENNIS	001	853	14	11,942	R	AC	0	46	87	88	\$10	1,731,674	SEAL CRACKS
DUBOIS LANE	S.F. CURB WALNUT 0+00	END OF PAVEMENT 2+07	DUBOIS	001	207	25	5,175	R	AC/AC	0	94	88	88	\$4	3,741,440	SEAL CRACKS
EAST ELM ST.	E.F.@ HWY1 0+00	W.F. CURB N FRANKLIN ST 3+25	EELM	001	325	44	14,300	C	AC	0	75	87	88	\$17	1,624,701	SEAL CRACKS
EAST LAUREL ST. 001	E.F. CURB HWY1 0+00	W.F. CURB HARRISON ST 9+25	ELAURE	001	925	45	41,625	C	AC	0	87	82	84	\$135	808,903	SEAL CRACKS
JEWETT ST.	S.F.SIDEWALK OAK ST.0+00	END OF CULDESAC 5+36	JEWETT	001	536	39	20,904	R	AC	0	72	72	74	\$117	601,446	SEAL CRACKS
JOHN CIMOLINO WAY	STEWART ST	END	JOHNCIMO	001	286	36	10,296	R	AC		49	87	88	\$9	1,731,674	SEAL CRACKS
LIVINGSTON ST.	S.F.CURB OAK 0+00	N.F.CURB WILLOW ST.6+17	LIVING	001	617	35	21,595	R	AC/AC	0	72	72	75	\$115	635,529	SEAL CRACKS
MAPLE ST. 001	S.HAROLD	ALLEY BETWN WHIPPLE HARRISON	MAPLE	001B	1,000	45	45,000	C	AC	0	69	87	88	\$51	1,624,701	SEAL CRACKS
MAPLE ST. 005	S. FRANKLIN	E.F. CURB HWY 1	MAPLE	005B	400	49	19,600	C	AC	0	81	76	78	\$93	661,126	SEAL CRACKS
N100E	E OAK ST	E ALDER ST	N100E	N100E1	413	16	6,608	O	AC		6	87	88	\$9	1,082,296	SEAL CRACKS

\*\* - Treatment from Project Selection

# Scenarios - Sections Selected for Treatment

Interest: 3.00%

Inflation: 3.00%

Printed: 1/28/2022

Scenario: 2021 S1: CITY'S EXISTING FUNDING (\$1.0M/YEAR)

## Year: 2028

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment
											Current PCI	PCI Before	PCI After			
NORTH HARBOR DRIVE 001	CITY LIMITS @GUARD RAIL 0+00	E.F.CURB S. FRANKLIN 19+90	NHARBO	001	1,990	28	55,720	R	AC	0	72	72	74	\$310	601,351	SEAL CRACKS
EAST OAK ST. 002	E.F. CURB McKINLEY ST. 0+00	BACK E.SIDE SDWLK. DANA 17+25	OAK	002	1,725	40	69,000	R	AC	0	77	76	78	\$319	657,931	SEAL CRACKS
S202F	MAPLE ST	105' N/O MAPLE ST	S202F	S202F1	105	14	1,470	O	AC		1	87	88	\$2	1,082,296	SEAL CRACKS
S500D	WALNUT ST	E CHESTNUT ST	S500D	S500D1	614	16	9,824	O	AC		94	86	87	\$39	407,127	SEAL CRACKS
SOUTH McPHERSON ST.	MAPLE	N.F. CURB CHESTNUT ST.	SMcPHE	003	1,042	44	45,848	C	AC	0	90	85	86	\$120	841,570	SEAL CRACKS
WALNUT ST.	GROVE ST	EAST END	WALNUT	004	188	43	8,084	R	AC/AC	0	51	87	88	\$7	1,731,674	SEAL CRACKS
WEST PINE ST.	END SIDE WALK N.SIDE 0+00	W.F. CURB HWY 1 2+58	WPINE	002	258	43	11,094	C	AC	0	80	75	77	\$56	633,847	SEAL CRACKS
<b>Treatment Total</b>													<b>\$1,413</b>			
<b>Year 2028 Area Total</b>									<b>667,703</b>	<b>Year 2028 Total</b>			<b>\$950,900</b>			

## Year: 2029

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment
											Current PCI	PCI Before	PCI After			
**AZALEA CIRCLE	W.F. CURB S. SANDERSON 0+00	END OF CULDESAC	AZALEA	001	275	38	10,450	R	AC	0	15	83	90	\$7,855	13,381	SLURRY SEAL W/ CRACK SEAL
CHIEF CELERI DR	W OAK ST	W ALDER ST	CELERI	001	475	15	7,125	A	AC		4	83	90	\$6,329	16,576	SLURRY SEAL W/ CRACK SEAL
CHESTNUT ST. 002	END CURB-GUTTER@001, 0+00	S. WHIPPLE	CHESTN	005	884	32	28,288	C	AC/AC	0	83	82	89	\$23,194	17,926	SLURRY SEAL W/ CRACK SEAL
CHESTNUT ST. 003	SANDERSON WY	END OF STREET (9+75)	CHESTN	015	975	32	31,200	C	AC/AC	0	73	74	82	\$25,582	20,487	SLURRY SEAL W/ CRACK SEAL
EAST LAUREL 002	E.F.CURB N.HARRISON 0+00	W.F.CURB N.HAROLD 10+05	ELAURE	002	1,005	40	40,200	C	AC/AC	0	75	76	84	\$32,961	21,818	SLURRY SEAL W/ CRACK SEAL
MAPLE ST. 005	ALLEY BETWN WHIPPLE HARRISON	S. FRANKLIN	MAPLE	005A	900	49	44,100	C	AC	0	68	82	90	\$36,159	15,531	SLURRY SEAL W/ CRACK SEAL
OLSEN LANE	S.F. CURB CHESTNUT 0+00	END OF ST. 6+52	OLSEN	001	652	28	18,256	R	AC/AC	0	94	82	89	\$13,722	16,046	SLURRY SEAL W/ CRACK SEAL
**PENITENTI WAY	W.F. CURB S.SANDERSON 0+00	END OF CULDESAC 2+75	PENITE	001	275	38	10,450	R	AC	0	16	0	42	\$7,855	4,074	SLURRY SEAL W/ CRACK SEAL
**SOUTH HAROLD ST. 003	S.F. CHESTNUT ST 0+00	END OF ST.@W.POWER POLE 5+00	SHAROL	003	620	16	9,920	R	AC	0	12	0	42	\$7,456	4,074	SLURRY SEAL W/ CRACK SEAL

\*\* - Treatment from Project Selection

# Scenarios - Sections Selected for Treatment

Interest: 3.00%

Inflation: 3.00%

Printed: 1/28/2022

Scenario: 2021 S1: CITY'S EXISTING FUNDING (\$1.0M/YEAR)

Year: 2029

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment
											Current PCI	PCI Before	PCI After			
SOUTH LINCOLN ST.	MAPLE ST	WILLOW ST	SLINCO	015A	282	30	8,460	C	AC	0	68	84	91	\$6,937	13,858	SLURRY SEAL W/ CRACK SEAL
WEST CYPRESS	WEST END	S MAIN ST	WCYPST	001	165	50	8,250	C	AC	0	51	84	91	\$6,765	13,858	SLURRY SEAL W/ CRACK SEAL
<b>Treatment Total</b>													<b>\$174,815</b>			
PARK ST. 003	END OF 002 0+00	END OF ST. 1+45 PARK		003	145	14	2,030	R	AC	0	66	64	74	\$3,191	7,184	MICROSURFACING W/ CRACK SEAL AND 5% DIGOUTS
<b>Treatment Total</b>													<b>\$3,191</b>			
N200D	E ALDER ST	E REDWOOD AVE	N200D	N200D1	405	18	7,290	O	AC		20	0	100	\$36,611	10,460	THICK MILL AND OVERLAY
EAST OAK ST. 003	BACK E.SIDE SDWLK DANA 0+00	CITY LIMIT SIGN OAK 16+10	OAK	003	1,610	24	38,640	R	AC	0	61	48	100	\$469,944	3,987	THICK MILL AND OVERLAY
<b>Treatment Total</b>													<b>\$506,555</b>			
N102G	300 N/O E OAK ST	E ALDER ST	N102G	N102G1	117	16	1,872	O	AC/AC		96	85	92	\$446	48,580	SINGLE CHIP SEAL
S600E	E END	WALNUT ST	S600E	S600E1	610	12	7,320	O	AC/AC		96	85	92	\$1,741	48,580	SINGLE CHIP SEAL
<b>Treatment Total</b>													<b>\$2,187</b>			
DELMAR DR	OCEAN VIEW DR	END SOUTH	DELM	002	242	30	7,260	R	AC		88	87	88	\$17	1,079,066	SEAL CRACKS
DICKWILLIAMS WAY	DANA ST	END AT CDS	DICKWI	001	185	28	5,180	R	AC		9	86	87	\$13	618,491	SEAL CRACKS
EAST ALDER 002	HAROLD	@HYDRANT END OF ST. 28+45	EALDER	002B	1,261	43	54,223	R	AC	0	81	78	80	\$232	662,377	SEAL CRACKS
EAST FIR ST. 002	E.F.CURB @ HWY 1 0+00	RR TRACKS	EFIR	002	142	44	6,248	C	AC	0	33	91	92	\$10	720,819	SEAL CRACKS
EAST FIR ST. 002	RR TRACKS	W.F.CURB H FRANKLIN	EFIR	003	192	44	8,448	C	AC/AC	0	92	86	87	\$14	2,256,949	SEAL CRACKS
EAST PINE ST.	CORRY ST	W.F. CURB HAROLD ST 19+30	EPINE	010	356	43	15,308	C	AC	0	79	87	88	\$18	1,577,379	SEAL CRACKS
EAST SPRUCE ST.	E.F. CURB HWY1 0+00	CEMETARY GATE 5+07	ESPRUC	001	507	45	22,815	R	AC	0	49	87	88	\$20	1,681,237	SEAL CRACKS
HAZEL ST.	E.F CURB HWY1 0+00	W.F.CURB S.FRANKLIN 3+40	HAZEL	001	340	32	10,880	R	AC	0	51	87	88	\$10	1,681,237	SEAL CRACKS
N100H	E OAK ST	E ALDER ST	N100H	N100H1	414	16	6,624	O	AC		2	87	88	\$9	1,050,773	SEAL CRACKS
N200F	E ALDER ST	E REDWOOD AVE	N200F	N200F1	407	14	5,698	O	AC		4	87	88	\$8	1,050,773	SEAL CRACKS
S500H	WALNUT ST	E CHESTNUT ST	S500H	S500H1	635	18	11,430	O	AC		94	85	87	\$49	399,289	SEAL CRACKS

\*\* - Treatment from Project Selection

# Scenarios - Sections Selected for Treatment

Interest: 3.00%

Inflation: 3.00%

Printed: 1/28/2022

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## Year: 2029

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment	
											Current PCI	PCI Before	PCI After				
SOUTH DANA ST.	N.F. CUEB CHESTNUT ST 0+00	S.F. CURB OAK ST. 19+00	SDANA	001	1,900	40	76,000	C	AC	0	91	84	86	\$219	817,550	SEAL CRACKS	
SOUTH SANDERSON ST. 001	N.F. CURB CHESTNUT ST 0+00	S.F. CURB OAK ST 19+15	SSANDE	001	1,915	40	76,600	C	AC	0	91	84	86	\$221	817,379	SEAL CRACKS	
<b>Treatment Total</b>													<b>\$840</b>				
<b>Year 2029 Area Total</b>									<b>580,565</b>		<b>Year 2029 Total</b>			<b>\$687,588</b>			

## Year: 2030

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment
											Current PCI	PCI Before	PCI After			
MAPLE ST. 005	S. FRANKLIN	E.F. CURB HWY 1	MAPLE	005B	400	49	19,600	C	AC	0	81	73	82	\$16,553	14,528	SLURRY SEAL W/ CRACK SEAL
NESS AVE.	E.F. CURB SANDERSON WAY 0+00	E. P.L. 111 NESS FENCE 2+45	NESS	001	245	22	5,390	R	AC	0	93	79	87	\$4,173	14,507	SLURRY SEAL W/ CRACK SEAL
NORTH HAROLD ST. 001	END OF PAVEMENT 0+00	N.S.DRVWY BEHIND M.SCHOOL3+16	NHAROL	001	316	28	8,848	R	AC/AC	0	94	81	88	\$6,850	16,272	SLURRY SEAL W/ CRACK SEAL
NORTH HAROLD ST. 002	N.SIDE M.S.SCH.DRVWAY 0+00	N.CURB @ FIR ST.2+85	NHAROL	002	285	40	11,400	R	AC/AC	0	94	81	88	\$8,826	16,272	SLURRY SEAL W/ CRACK SEAL
WALL ST. 001	WILLOW	N.F. CURB@ALLEY ST 13+50	WALL	001B	650	38	24,700	R	AC/AC	0	79	80	87	\$19,122	18,259	SLURRY SEAL W/ CRACK SEAL
WOODWARD ST.	S EDGE SOUTH ST. 0+00	END ST. @ NAIL 6+50	WOODWA	001	650	26	16,900	R	AC/AC	0	94	81	88	\$13,083	16,272	SLURRY SEAL W/ CRACK SEAL
WEST PINE ST.	END SIDE WALK N.SIDE 0+00	W.F. CURB HWY 1 2+58	WPINE	002	258	43	11,094	C	AC	0	80	72	81	\$9,370	14,197	SLURRY SEAL W/ CRACK SEAL
<b>Treatment Total</b>													<b>\$77,977</b>			
WEST ELM ST.	W.F. CURB HWY1 0+00	W.EDGE PAVEMENT 10+30	WELM	001	1,030	43	44,290	C	AC	0	73	65	74	\$85,717	5,466	RUBBERIZED CHIP SEAL+MICROSURFACING W/ 3% DIGOUTS
<b>Treatment Total</b>													<b>\$85,717</b>			
HAZELWOOD ST.	S.F. SOUTH ST. 0+00	HYDRANT @ END OF ST.3+60	HAZELW	001	360	28	10,080	R	AC	0	64	63	74	\$11,351	12,473	MICROSURFACING W/ CRACK SEAL AND 3% DIGOUTS
<b>Treatment Total</b>													<b>\$11,351</b>			

\*\* - Treatment from Project Selection

# Scenarios - Sections Selected for Treatment

Interest: 3.00%

Inflation: 3.00%

Printed: 1/28/2022

Scenario: 2021 S1: CITY'S EXISTING FUNDING (\$1.0M/YEAR)

Year: 2030

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment
											Current PCI	PCI Before	PCI After			
MADRONE ST. 001	E.F.CURB @ HWY 1 0+00	W.F.CURB @ McPHERSON ST.6+50	MADRON	001	650	43	27,950	R	AC	0	67	63	73	\$45,242	6,941	MICROSURFACING W/ CRACK SEAL AND 5% DIGOUTS
MANZANITA ST.	W.F. CURB N. FRANKLIN ST.0+00	E.F. CURB HWY1 3+30	MANZAN	001	330	26	8,580	R	AC	0	69	68	77	\$13,889	7,191	MICROSURFACING W/ CRACK SEAL AND 5% DIGOUTS
WILLOW ST. 001	W.F.CURB S.SANDERSON 0+00	E.EDGE ALLEY LIVINGSTON 6+23	WILLOW	001	623	32	19,936	R	AC	0	67	63	73	\$32,270	6,941	MICROSURFACING W/ CRACK SEAL AND 5% DIGOUTS
<b>Treatment Total</b>													<b>\$91,401</b>			
N200H	E ALDER ST	E REDWOOD AVE	N200H	N200H1	424	14	5,936	O	AC		2	0	100	\$30,705	10,155	THICK MILL AND OVERLAY
<b>Treatment Total</b>													<b>\$30,705</b>			
NORTH HAROLD ST. 005	ALDER ST	N.F. CURB OAK ST	NHAROL	007	400	52	20,800	C	AC	0	58	23	100	\$345,463	3,531	8" FDR W/ 3.5" HMA
<b>Treatment Total</b>													<b>\$345,463</b>			
HOLMES LANE	E.F. CURB OLD HAUL ROAD 0+00	W.F. CURB STEWART ST. 5+60	HOLMES	001	560	36	20,160	R	AC	0	35	10	100	\$324,192	3,069	4.5" SURFACE RECONSTRUCT
<b>Treatment Total</b>													<b>\$324,192</b>			
S100H	MADRONE ST	E OAK ST	S100H	S100H1	410	14	5,740	O	AC		96	82	89	\$2,424	29,849	SINGLE CHIP SEAL
S300G	HAZEL ST	MAPLE ST	S300G	S300G1	373	16	5,968	O	AC		96	82	89	\$2,521	29,849	SINGLE CHIP SEAL
S300K	S END	MAPLE ST	S300K	S300K1	579	13	7,527	O	AC		96	82	89	\$3,179	29,849	SINGLE CHIP SEAL
<b>Treatment Total</b>													<b>\$8,124</b>			
**BOATYARD DR. 002	N.SIDE DRIVE WY HARVEST 0+00	N.F. HWY20 7+85	BOATYA	002	785	41	32,185	A	AC/AC	0	48	85	86	\$64	1,635,535	SEAL CRACKS
**CHESTNUT ST. 001	E.F. CURB HWY1 0+00	END OF CURB-GUTTER S. SIDEWALK	CHESTN	001	498	40	19,920	A	AC	0	5	83	85	\$65	968,868	SEAL CRACKS
EDDY ST.	W.F. CURB S SANDERSON 0+00	END OF STREET 3+18	EDDY	001	318	41	13,038	R	AC	0	82	78	80	\$59	634,764	SEAL CRACKS
HARBOR AVE	OCEAN VIEW DR	NORTH END	HARAV	002	140	32	4,480	R	AC/AC	0	32	86	87	\$12	600,476	SEAL CRACKS
MAPLE ST. 001	W.F.CURB S.LINCOLN 0+00	S.HAROLD	MAPLE	001A	640	45	28,800	C	AC/AC	0	95	87	88	\$28	5,141,327	SEAL CRACKS
N100J	E OAK ST	E ALDER ST	N100J	N100J1	418	16	6,688	O	AC		0	87	88	\$10	1,020,168	SEAL CRACKS
N401G	214' N/O LAUREL ST	E PINE ST	N401G	N401G1	206	14	2,884	O	AC		20	87	88	\$4	1,020,168	SEAL CRACKS
N800C	W SPRUCE ST	W ELM ST	N800C	N800C1	422	16	6,752	O	AC		94	85	86	\$32	396,179	SEAL CRACKS

\*\* - Treatment from Project Selection



# Scenarios - Sections Selected for Treatment

Interest: 3.00%

Inflation: 3.00%

Printed: 1/28/2022

Scenario: 2021 S1: CITY'S EXISTING FUNDING (\$1.0M/YEAR)

Year: 2030

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment	
											Current PCI	PCI Before	PCI After				
NANCY WAY	N.F.CURB CEDAR 0+00	END OF PAVEMENT @ OSPREY 2+66	NANCY	001	266	31	8,246	R	AC	0	54	87	88	\$8	1,632,269	SEAL CRACKS	
NORTH CORRY ST.	N.F.CURB OAK ST.0+00	S.F.CURB FIR ST. 22+47	NCORRY	001	2,247	42	94,374	R	AC	0	79	75	77	\$485	613,155	SEAL CRACKS	
NORTH HAROLD ST. 005	S.F. CURB FIR ST 0+00	LAUREL ST	NHAROL	005	920	52	47,840	C	AC	0	80	87	88	\$58	1,531,436	SEAL CRACKS	
OCEAN VIEW DRV.EAST 003	E.F. CURB HWY 1 0+00	5+30 FROM HWY1	OCEANV	003	530	26	13,780	R	AC	0	82	78	80	\$63	634,819	SEAL CRACKS	
PARK ST. 002	S.F. CURB WILLOW 0+00	WHERE ST.NARROWS 300 BLK 5+68	PARK	002	568	28	15,904	R	AC	0	81	77	79	\$76	630,733	SEAL CRACKS	
SOUTH McPHERSON ST.	S.F. CURB OAK ST.	MADRONE	SMcPHE	001	436	44	19,184	C	AC	0	94	85	86	\$54	794,334	SEAL CRACKS	
STEWART ST. 002	S.F. CURB W. ELM 0+00	SPRUCE	STEWAR	002	413	43	17,759	R	AC/AC	0	53	87	88	\$16	1,632,269	SEAL CRACKS	
SOUTH WHIPPLE ST. 002	N.SIDE HAZEL RT OF WAY 0+00	N.F. CURB WALNUT ST.12+85	SWHIPP	002	1,285	32	41,120	R	AC	0	82	78	80	\$186	634,764	SEAL CRACKS	
**WEST FIR ST.	E.F.@ ALLEY 0+00	W.F. HWY1 8+25	WFIR	001	900	43	38,700	C	AC	0	3	88	89	\$85	785,001	SEAL CRACKS	
<b>Treatment Total</b>												<b>\$1,305</b>					
<b>Year 2030 Area Total</b>									<b>686,553</b>	<b>Year 2030 Total</b>			<b>\$976,235</b>				

Year: 2031

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment
											Current PCI	PCI Before	PCI After			
ALGER ST.	N.F.CURB LAUREL 0+00	END OF ST. 5+64	ALGER	001	564	22	12,408	R	AC/AC	0	94	79	87	\$9,894	16,303	SLURRY SEAL W/ CRACK SEAL
EAST ELM ST.	E.F.@ HWY1 0+00	W.F. CURB N FRANKLIN ST 3+25	EELM	001	325	44	14,300	C	AC	0	75	82	90	\$12,439	14,639	SLURRY SEAL W/ CRACK SEAL
EAST FIR ST. 002	E.F.CURB @ HWY 1 0+00	RR TRACKS	EFIR	002	142	44	6,248	C	AC	0	33	88	94	\$5,435	13,327	SLURRY SEAL W/ CRACK SEAL
EAST FIR ST. 002	RR TRACKS	W.F.CURB H FRANKLIN	EFIR	003	192	44	8,448	C	AC/AC	0	92	84	91	\$7,349	15,005	SLURRY SEAL W/ CRACK SEAL
HALSEY WAY	E.F. CURB McKINLEY ST.0+00	E.F. CURB HALSY WAY 2+85	HALSEY	001	285	32	9,120	R	AC/AC	0	94	79	87	\$7,272	16,303	SLURRY SEAL W/ CRACK SEAL
NORTH SANDERSON WAY 002	N.F. CURB CEDAR ST.0+00	S. P.L. 222 N SANDERSON 7+27	NSANDE	002	727	20	14,540	R	AC/AC	0	94	79	87	\$11,594	16,303	SLURRY SEAL W/ CRACK SEAL

\*\* - Treatment from Project Selection

# Scenarios - Sections Selected for Treatment

Interest: 3.00%

Inflation: 3.00%

Printed: 1/28/2022

Scenario: 2021 S1: CITY'S EXISTING FUNDING (\$1.0M/YEAR)

Year: 2031

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment
											Current PCI	PCI Before	PCI After			
SOUTH DANA ST.	N.F. CUEB CHESTNUT ST 0+00	S.F. CURB OAK ST. 19+00	SDANA	001	1,900	40	76,000	C	AC	0	91	82	89	\$66,109	15,236	SLURRY SEAL W/ CRACK SEAL
SOUTH McPHERSON ST.	MAPLE	N.F. CURB CHESTNUT ST.	SMcPHE	003	1,042	44	45,848	C	AC	0	90	80	88	\$39,881	15,267	SLURRY SEAL W/ CRACK SEAL
SNUG HARBOR PLACE	OCEAN VIEW DR	SOUTH END	SNUGH	001	110	19	2,090	R	AC		93	79	87	\$1,667	14,014	SLURRY SEAL W/ CRACK SEAL
SPRING ST.	S.F. CURB CHESTNUT ST 0+00	END OF PAVEMENT 5+04	SPRING	001	504	30	15,120	R	AC/AC	0	94	79	87	\$12,057	16,300	SLURRY SEAL W/ CRACK SEAL
<b>Treatment Total</b>													<b>\$173,697</b>			
BRANDON WAY	N.F. CURB E. FIR ST. 0+00	GUARD BARRIER @ WINIFRED 7+28	BRANDO	001	728	34	24,752	R	AC	0	84	70	79	\$28,708	10,136	MICROSURFACING W/ CRACK SEAL AND 3% DIGOUTS
NORTH WHIPPLE ST.	ALDER ST	S.F.CURB FIR ST.	NWHIPP	002	1,850	44	81,400	R	AC	0	84	70	79	\$94,408	10,136	MICROSURFACING W/ CRACK SEAL AND 3% DIGOUTS
<b>Treatment Total</b>													<b>\$123,116</b>			
JEWETT ST.	S.F.SIDEWALK OAK ST.0+00	END OF CULDESAC 5+36	JEWETT	001	536	39	20,904	R	AC	0	72	69	78	\$34,852	7,023	MICROSURFACING W/ CRACK SEAL AND 5% DIGOUTS
LIVINGSTON ST.	S.F.CURB OAK 0+00	N.F.CURB WILLOW ST.6+17	LIVING	001	617	35	21,595	R	AC/AC	0	72	70	79	\$36,004	7,128	MICROSURFACING W/ CRACK SEAL AND 5% DIGOUTS
NORTH HARBOR DRIVE 001	CITY LIMITS @GUARD RAIL 0+00	E.F.CURB S. FRANKLIN 19+90	NHARBO	001	1,990	28	55,720	R	AC	0	72	69	78	\$92,897	7,023	MICROSURFACING W/ CRACK SEAL AND 5% DIGOUTS
<b>Treatment Total</b>													<b>\$163,753</b>			
N200J	E ALDER ST	E REDWOOD AVE	N200J	N200J1	425	16	6,800	O	AC		0	0	100	\$36,230	9,860	THICK MILL AND OVERLAY
<b>Treatment Total</b>													<b>\$36,230</b>			
SOUTH McPHERSON ST.	MADRONE	MAPLE	SMcPHE	002	422	44	18,568	C	AC/AC	0	51	23	100	\$317,644	3,428	8" FDR W/ 3.5" HMA
<b>Treatment Total</b>													<b>\$317,644</b>			
STEWART CIRCLE	W.F. CURB STEWART 0+00	END OF CIRCLE 0+95	STEWCI	001	95	90	9,000	R	AC	0	37	9	100	\$149,071	2,979	4.5" SURFACE RECONSTRUCT
<b>Treatment Total</b>													<b>\$149,071</b>			
N100D	E OAK ST	E ALDER ST	N100D	N100D1	417	20	8,340	O	AC		21	82	90	\$2,104	42,703	SINGLE CHIP SEAL
N102C	N101C	MAIN ST	N102C	N102C1	146	26	3,796	O	AC		16	82	90	\$958	42,703	SINGLE CHIP SEAL
N300E	E REDWOOD AVE	E LAUREL ST	N300E	N300E1	416	18	7,488	O	AC		94	81	88	\$3,257	24,702	SINGLE CHIP SEAL

\*\* - Treatment from Project Selection

# Scenarios - Sections Selected for Treatment

Interest: 3.00%

Inflation: 3.00%

Printed: 1/28/2022

Scenario: 2021 S1: CITY'S EXISTING FUNDING (\$1.0M/YEAR)

Year: 2031

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surface Type	Area ID	Treatment			Cost	Rating	Treatment
											Current PCI	PCI Before	PCI After			
S100J	MADRONE ST	E OAK ST	S100J	S100J1	418	14	5,852	O	AC		94	81	88	\$2,546	24,701	SINGLE CHIP SEAL
S301N	S302N	WILLOW ST	S301N	S301N1	652	12	7,824	O	AC		94	81	88	\$3,403	24,716	SINGLE CHIP SEAL
S302N	S301N	WALL ST	S302N	S302N1	139	16	2,224	O	AC		94	81	88	\$968	24,701	SINGLE CHIP SEAL
<b>Treatment Total</b>													<b>\$13,236</b>			
BOATYARD DR. 001	5+30 FORM HWY1 0+00	4+32 FROM BEGIN	BOATYA	001	432	24	10,368	A	AC/AC	0	92	87	88	\$14	3,876,754	SEAL CRACKS
EAST ALDER 001	E.F CURB HWY 1 0+00	W.F.CURB N.FRANKLIN ST. 3+40	EALDER	001	340	43	14,620	C	AC	0	83	87	88	\$37	1,118,531	SEAL CRACKS
EAST CYPRESS ST. 001	END OF KEMPPE @ VALVE BOX 0+0	W.F.CURB RIVER DRV 12+96	ECYPRES	001	1,296	40	51,840	C	AC/AC	0	81	88	89	\$29	5,084,339	SEAL CRACKS
ESPEY WAY	S.F. CURB WALNUT ST 0+00	END OF PAVEMENT 2+58	ESPEY	001	258	36	9,288	R	AC/AC	0	94	86	87	\$17	1,673,117	SEAL CRACKS
FLORENCE ST.	S.F. CURB OAK 0+00	N.F. CURB WILLOW ST. 6+03	FLOREN	001	603	31	18,693	R	AC	0	82	76	79	\$94	607,731	SEAL CRACKS
KEMPPE WAY	433 FT E/O HOSPITAL WY	N. OF VALVE BOX 9+35	KEMPPE	010	502	37	18,574	R	AC	0	59	87	88	\$17	1,584,727	SEAL CRACKS
N402G	E LAUREL ST	214' N/O LAUREL ST	N402G	N402G1	214	14	2,996	O	AC		0	87	88	\$5	990,454	SEAL CRACKS
NORTH HAROLD ST. 006	LAUREL ST	ALDER ST	NHAROL	006	920	52	47,840	C	AC/AC	0	74	84	85	\$121	2,188,280	SEAL CRACKS
OAK TERRACE COURT	N.F. CURB OAK ST. 0+00	END OF ST. 3+38	OAKTER	001	338	36	12,168	R	AC	0	85	79	81	\$54	625,997	SEAL CRACKS
PARK ST. 001	S.F. CURB OAK 0+00	N.F.CURB WILLOW 6+00	PARK	001	600	25	15,000	R	AC/AC	0	55	87	88	\$14	1,584,727	SEAL CRACKS
PUDDING CREEK RD.	C.L SIGN0+00	E.F. HWY1 @ LIMIT LINE 9+55	PUDDIN	001	955	23	21,965	R	AC	0	89	82	84	\$79	905,115	SEAL CRACKS
SOUTH ST. 005	E. EDGE VALLEY GUTTER HWY 1	HAZELWOOD ST	SOUTH	005	1,218	38	46,284	C	AC	0	80	87	88	\$57	1,486,831	SEAL CRACKS
SOUTH SANDERSON ST. 002	412 FT N/O END OF W. SIDEWALK	S.F. CURB CHESTNUT ST 1040	SSANDE	010	628	30	18,840	R	AC	0	84	78	80	\$87	623,003	SEAL CRACKS
<b>Treatment Total</b>													<b>\$625</b>			
<b>Year 2031 Area Total</b>									<b>766,861</b>	<b>Year 2031 Total</b>			<b>\$977,372</b>			
<b>Grand Total Section Area:</b>									<b>5,786,903</b>	<b>Grand Total</b>			<b>\$9,371,625</b>			

\*\* - Treatment from Project Selection